

Traffix Group

Traffic Engineering Assessment

Proposed Commercial Development
Cathedral Precinct Redevelopment, Bendigo

Prepared for
Catholic Diocese of Sandhurst

August 2024

G31621R-011



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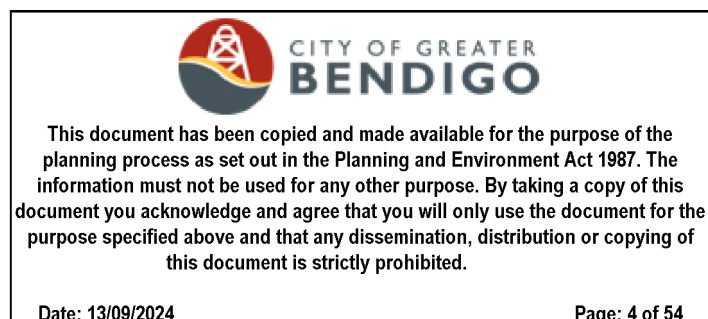
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1. Introduction

Traffix Group has been engaged by Catholic Diocese of Sandhurst to undertake a Traffic Engineering Assessment for the Cathedral Precinct Redevelopment, Bendigo.

In particular, this report provides a detailed traffic engineering assessment of the parking and access arrangements of the proposed development.

2. Proposal

The proposal is for a commercial development on the south-eastern portion of the Cathedral. The proposal includes the following:

- 1,777m² of office. The offices will predominantly serve the Catholic Diocese of Sandhurst and Catholic Education Sandhurst Ltd. These are related to the use of the Sacred Heart Cathedral.
- A 'Chocolaterie' which will consist of:
 - An 852m² shop, and
 - A 566m² café
- A small corner retail tenancy of 198m²
- A 130 patron place of assembly (this currently exists at the location of the proposed development, and will be incorporated into the new development, however the patron numbers will be increased from 80, which is an increase of 50 patrons).
- 35 x car spaces, accessed via a crossover to Short Street.
- 39 x bicycle spaces.

Car parking is proposed to be allocated to the office during business hours (with the exception of 2 spaces that will be allocated to 'Chocolaterie' staff), however parking will be available to staff of the 'Chocolaterie' on weekends.

There will be no change to the existing Cathedral on the site, which will remain post-development, however the parish hall will be replaced with a hall of similar capacity.

The public pedestrian path in the vicinity of the crossover will also be re-aligned to reduce the gradient along the site crossover and create a safer interface between pedestrians and vehicles. This crossover and footpath will be reconstructed to the satisfaction of Council.

There will be no net change to car parking conditions along Short Street post-development (18 car spaces will continue to be available).

Loading is proposed on-site via a dedicated loading bay, which will be accessed via the site carpark entrance.

Waste collection will be collected on-street via the Short Street kerbside, however bins will be stored inside the building.

A copy of the development plans prepared by Bickerton Masters (dated July, 2024) are attached at Appendix A.

3. Existing Conditions

3.1. Subject Site

The subject site is located at 46-50 High Street, Bendigo. The table below summarises the key characteristics of the subject site.


Table 1: Subject Site Description

Characteristic	Description
Address	46-50 High Street, Bendigo
Area	3,540m ²
Frontages	67.93m to High Street 77.39m to Short Street
Zoning and relevant traffic overlays	Commercial 1 Zone – C1Z Parking Overlay – Precinct 1 (PO1)
Activity Centre	Bendigo City Centre
Current use of site	Mostly vacant land, with a retail building shell and a currently vacant hall
Vehicle access	Single width crossover to Short Street Two unused crossovers to High Street
On-street parking along site frontage (including parking along associated church frontage)	A total of 77 publicly available parking as follows: <ul style="list-style-type: none"> • 12 car spaces along High Street • 18 car spaces along Short Street • 31 car spaces along Mackenzie Street • 16 car spaces along Wattle Street

A locality plan, aerial photograph and land use zoning map is provided at Figure 1 to Figure 3.

Significant nearby land uses include:

- **Bendigo Art Gallery** located 450m northeast.
- **Girton Grammar School** located 350m west.
- **The Capital** located 500m northeast.
- **Rosalind Park** located 500m northeast.
- **Bendigo Market Place** located 850m southeast.




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Figure 1: Locality Plan (Source: Melway)



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Figure 2: Aerial Photograph (Source: Nearmap)

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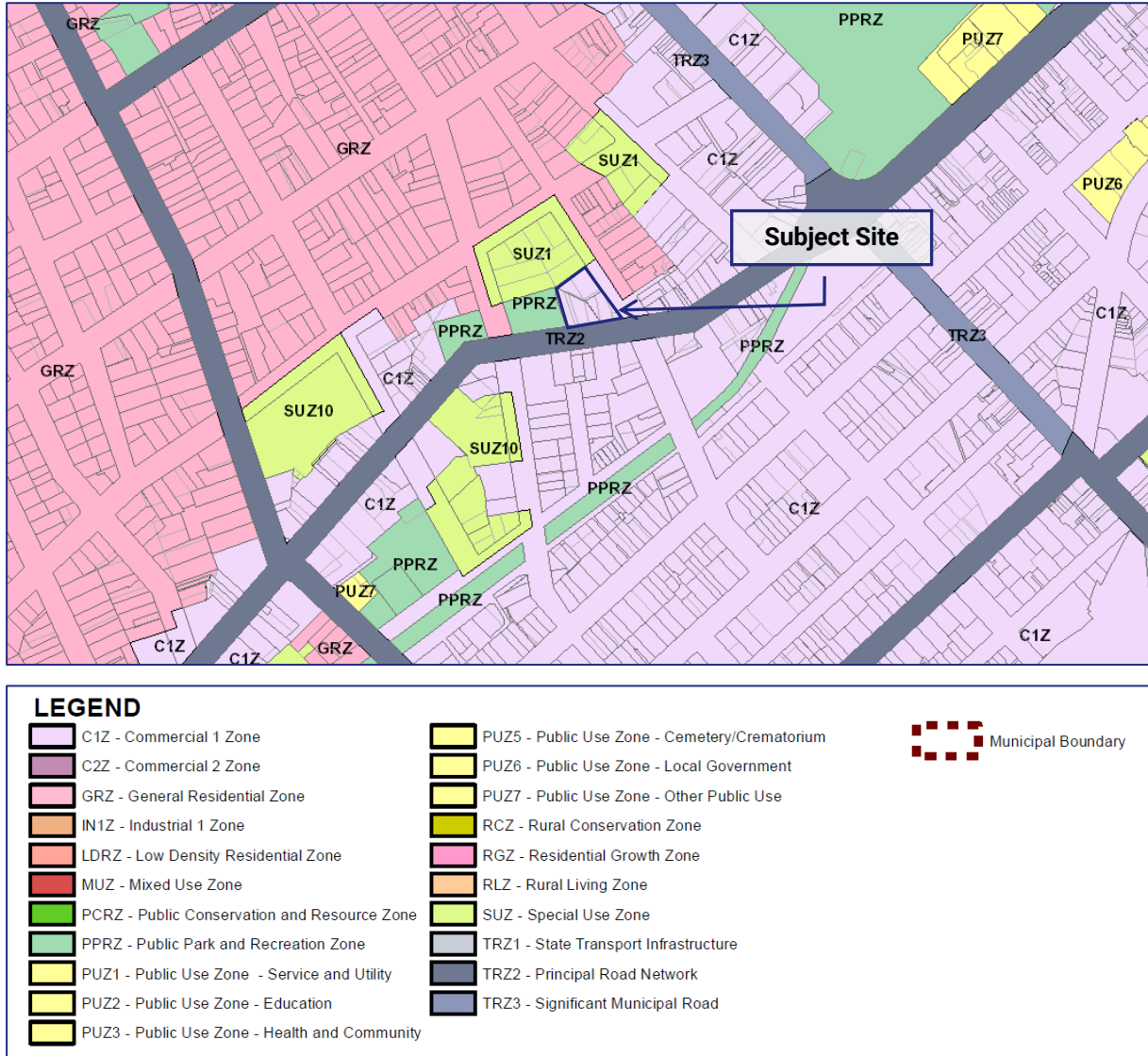



Figure 3: Land Use Zoning Map (Source: Planning Schemes Online)



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Traffic Engineering Assessment



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3.2. Road Network

A summary of the local road network is provided in the table below.

Photos of the surrounding road network are presented following the table.

Table 2: Local Road Network

Road Name	Agency	Classification	Transport Zone	Configuration	Speed Limit	On-Street Parking
High Street/Midland Highway	DTP	Arterial Road	TRZ2	5 traffic lanes Undivided carriageway Shared tram tracks and right hand turning lane in centre lane	Changes between 50km/h and 60km/h along site frontage	Mostly short-term (2P)
Mackenzie Street	Council	Local Road	No	7.8m carriageway with parallel parking on the north side, angled parking on the south	50km/h	Mixture of time restricted and unrestricted parking both sides
Wattle Street	Council	Collector Road	No	9.4m carriageway with angled parking on the both sides	50km/h	Mixture of time restricted and unrestricted parking both sides
Short Street	Council	Collector Road	No	7.1m carriageway with parallel parking on the west side, angled parking on the east	50km/h	Mixture of time restricted and unrestricted parking both sides

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Figure 4: High Street – view east



Figure 5: High Street – view west



Figure 6: Mackenzie Street – view east



Figure 7: Mackenzie Street – view west



Figure 8: Wattle Street – view north




Figure 9: Wattle Street – view south



Figure 10: Short Street – view north



Figure 11: Short Street – view south

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3.3. Car Parking Conditions

Traffic Group has completed parking surveys of on-street parking in the vicinity of the subject site. The purpose of the surveys was to assess the supply, management and demand for public parking resources in the nearby area. The surveys were completed at hourly intervals between the following times:

- 10am-3pm on Friday 5th May, 2023, and
- 10am-3pm on Saturday 6th May, 2023.

These times correspond to the peak demand times for the proposed uses on the site and the nearby area.

The detailed parking surveys are presented at Appendix B.

The survey area is presented in the figure below, which comprises an area of approximately 200m around the subject site.

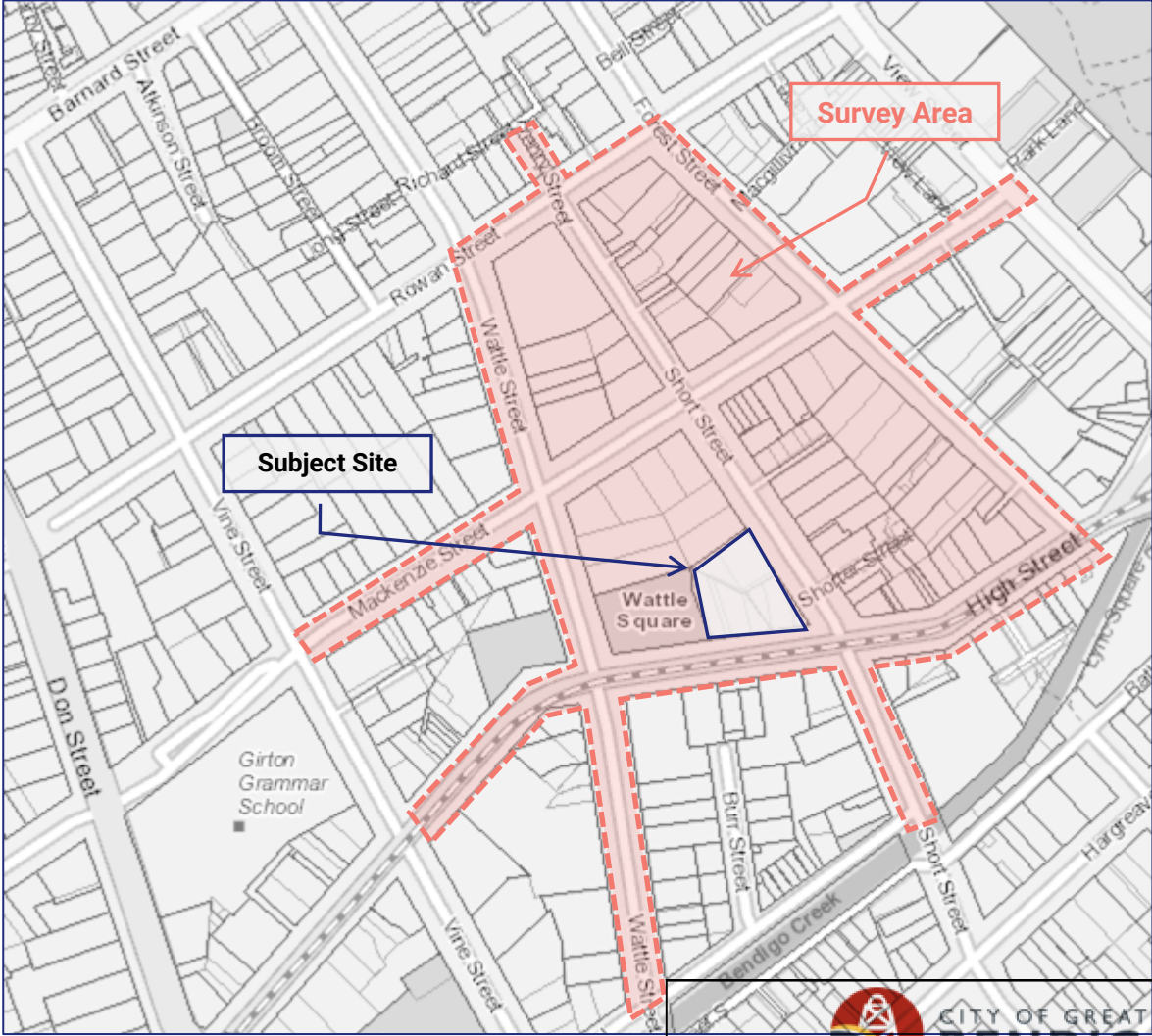


Figure 12: Parking Survey Inventory (Source: VicPlan)

Traffic Engineering Assessment

There are 77 publicly available parking spaces on the block of land which includes the site, associated church, and park including:

- 12 car spaces along High Street, including 11 x '2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat' spaces, and 1 x 'P10 pick up and drop off only' space
- 18 car spaces along Short Street, including 9 unrestricted spaces, and 9 x '2P 8:30am-5:30pm Mon-Fri' space
- 31 unrestricted car spaces along Mackenzie Street
- 16 car spaces along Wattle Street, subject to '1 1/2P 9am-5:30pm Mon-Fri'

The car parking surveys identified between 560-564 car spaces available for use by the general public in the nearby area¹. Car Parking was generally a mixture of unrestricted and short term (1/2P, 1P and 2P) parking.

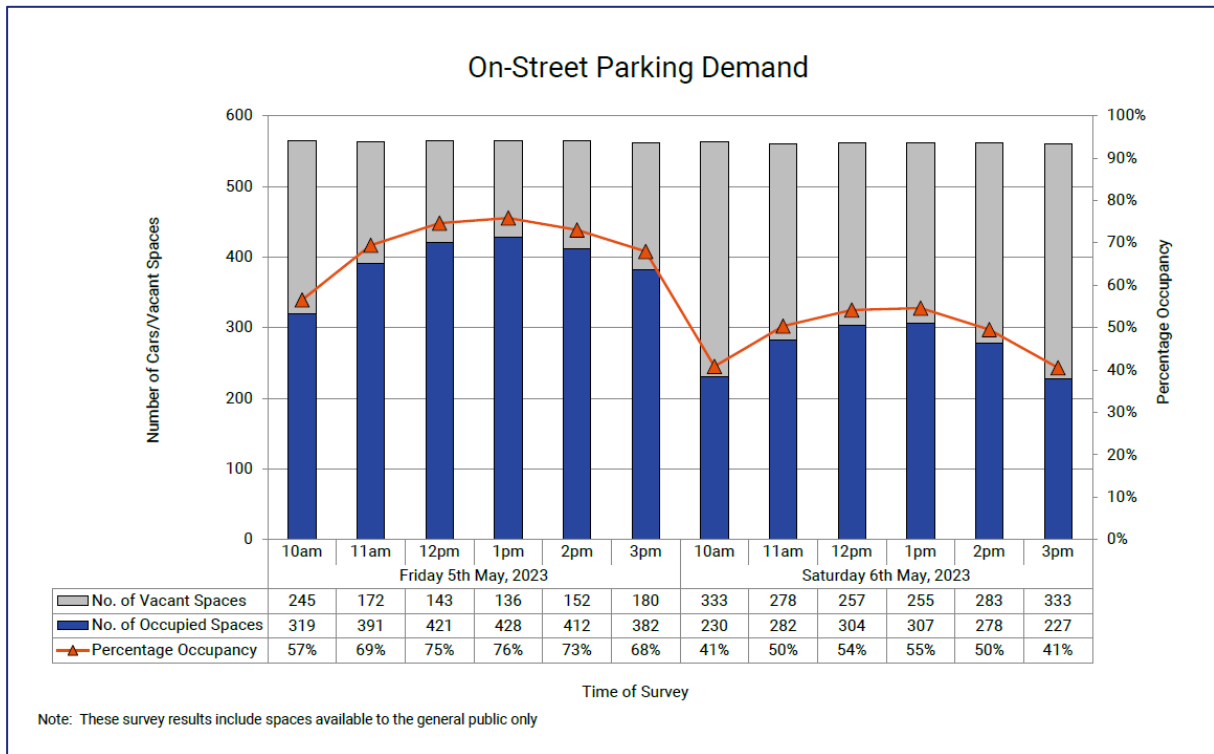


Figure 13: Profile of On-Street Parking Demand

Overall demand for on-street parking was moderate over the surveyed period. A minimum of 136 vacant spaces were recorded over the survey period (76% occupancy), which occurred at 1pm on Friday 5th May, 2023.

Demand for car parking along the site's frontages to Wattle Street, Short Street, High Street and Mackenzie Street was also low to moderate (see Appendix B for demands broken down by specific streets). This parking is a mixture of short-term and long-term parking.

¹ Includes all car spaces available to the general public, excluding those subject to 'No Stopping', 'Loading Zone', 'Clearway' and 'Bus Zone' restrictions during the relevant enforcement period.

Car parking demands along the commercial areas of High Street were also generally low.

There are a total of 128 unrestricted car spaces available in areas that do not front residential properties, including the site's frontage, along Forest Street, along Wattle Street and Short Street (to the south of High Street), and sections of MacKenzie Street (in locations where there are non-residential properties). On average, 40 of these spaces were vacant.

In addition to the on-street car parking, there are also numerous off-street parking areas nearby to the site. The nearest of these is a Council multi-storey carpark, located 500m south-east of the site, on the corner of Queen Street and Edwards Street.

This carpark includes 420 car spaces, which are generally free for the first 90 minutes, and ticketed parking for any further length of time. Of these spaces 399 are publicly available, and allow for long-term parking.

3.4. Public Transport

The site is located in close proximity to numerous bus services, with Bendigo Station and Mitchell Street bus stop hub also being within walking distance to the site.

These central bus stops serve all of Bendigo, and travel to Eaglehawk, Epsom, Goornong, Kangaroo Flat, Spring Gully, Latrobe University, Strathfieldsaye, Heathcote and Maiden Gully.

A summary is provided at Table 3 and map of the broader services provided at Figure 14.

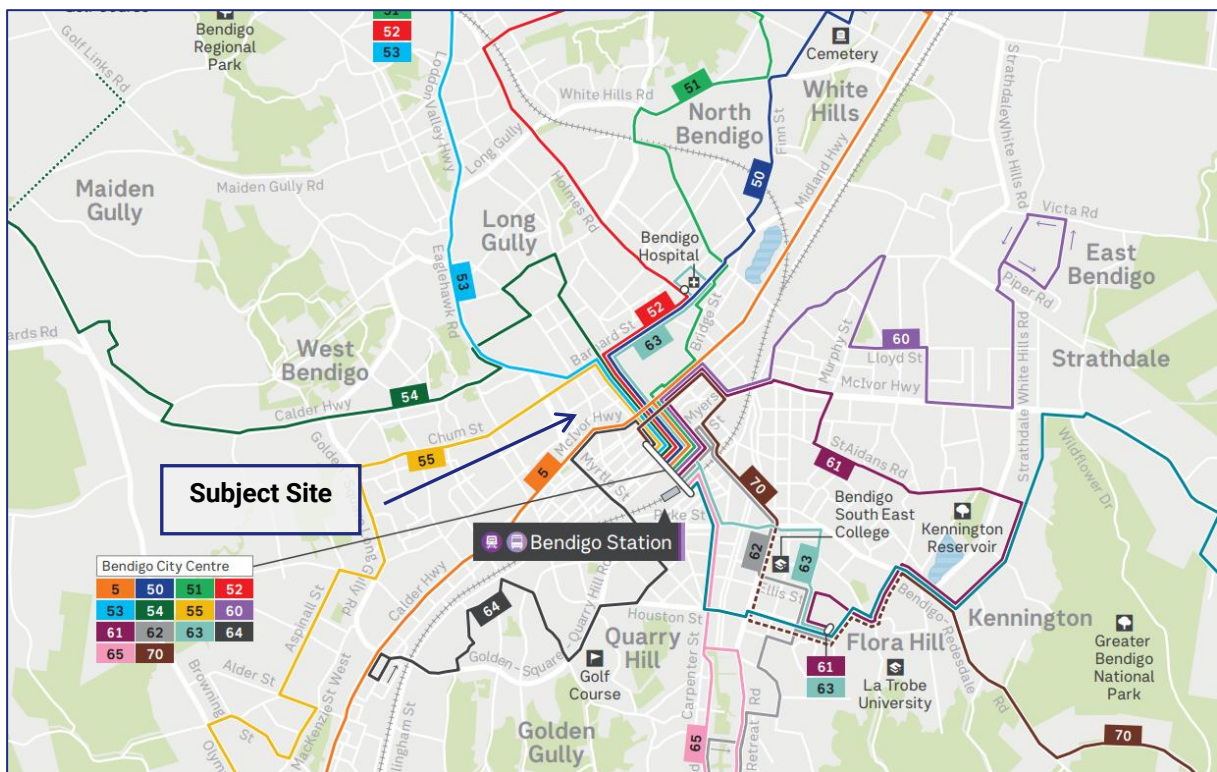


Figure 14: Public Transport Map (Source: PTV)

Table 3: Summary of Public Transport Services

Service	Between	Via
Midland Highway – operates past the site		
Bus Route 5	Huntly & Kangaroo Flat	Bendigo Station
Bus Route 64	Bendigo Station & Golden Square	Quarry Hill
Mitchell Street – operates approximately 400m east of the site (6 minute walk)		
Bus Route 50	Bendigo Station & Epsom Station	Goynes Road
Bus Route 51	Bendigo Station & Eaglehawk	Jackass Flat
Bus Route 52	Bendigo Station & Eaglehawk	Arnold Street
Bus Route 53	Bendigo Station & Eaglehawk	Eaglehawk Road
Bus Route 54	Bendigo Station & Maiden Gully	Calder Highway
Bus Route 55	Bendigo Station & Kangaroo Flat	Golden Square
Bus Route 60	Bendigo Station & East Bendigo	Strickland Road
Bus Route 61	Bendigo Station & La Trobe University	Strathdale
Bus Route 62	Bendigo Station & Spring Gully	La Trobe University
Bus Route 63	Bendigo Hospital & La Trobe University	Bendigo Station
Bus Route 65	Bendigo Station & Spring Gully	Carpenter Street
Bus Route 70	Bendigo Station & Strathfieldsaye	Kennington
Bendigo Railway Station – operates approximately 1km south-east of the site (15 minute walk)		
Bendigo Line	Bendigo & Melbourne	Castlemaine and Sunbury



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4. Car Parking Considerations

4.1. Statutory Car Parking Assessment

The proposed development falls under the land-use categories of 'office', 'shop', 'food and drink premises' (café) and 'place of assembly' under Clause 73.03 of the Planning Scheme. The Planning Scheme sets out the parking requirements for new developments under Clause 52.06. The purpose of Clause 52.06 is:

- *To ensure that car parking is provided in accordance with the Municipal Planning Strategy and the Planning Policy Framework.*
- *To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated, the activities on the land and the nature of the locality.*
- *To support sustainable transport alternatives to the motor car.*
- *To promote the efficient use of car parking spaces through the consolidation of car parking facilities.*
- *To ensure that car parking does not adversely affect the amenity of the locality.*
- *To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.*

The statutory parking requirements are set out at Clause 52.06-5 of the Planning Scheme. Clause 52.06-5 states:

Column A applies unless Column B applies.

Column B applies if:

- *any part of the land is identified as being within the Principal Public Transport Network Area as shown on the Principal Public Transport Network Area Maps (State Government of Victoria, 2018); or*
- *a schedule to the Parking Overlay or another provision of the planning scheme specifies that Column B applies.*

The site is located within a Parking Overlay – Schedule 1 (PO1). This parking overlay applies the following car parking rates that are applicable to the site.

- Office – 2.5 car spaces per 100m² LFA
- Shop – 3.5 car spaces per 100m² LFA

PO1 also states that for all uses not listed in the table, the Column B rates of Clause 52.06-5 apply.

Clause 52.06-5 states the following:

Where an existing use is increased by the measure specified in Column C of Table 1 for that use, the car parking requirement only applies to the increase, provided the existing number of car parking spaces currently being provided in connection with the existing use is not reduced.



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The existing parish accommodates approximately 80 persons currently, though is not capped at any specific number. The previous permit granted for the site allowed for the patronage to be capped at 120 persons with no additional car parking provided. This proposal seeks a cap of 130 persons with no additional car parking provided. The existing use right of a parish hall will be lost through the demolition of the existing hall and accordingly, we have conservatively assumed that the parish is a 'new' use for the purposes of the statutory assessment.

The statutory car parking assessment of the development is set out in Table 4 below.

Table 4: Statutory Car Parking Assessment – Schedule 1 to Clause 45.09

Use	Size / No.	Statutory Parking Rate	Parking Requirement ⁽¹⁾	Parking Provision	Shortfall / Surplus
Office	1,777m ²	2.5 car spaces per 100m ² NFA	44	33	-11
Shop	1,050m ²	3.5 car spaces per 100m ² LFA	36	2	-34
Food and Drink Premises	566m ²	3.5 car spaces per 100m ² LFA	19	0	-19
Place of Assembly	130	0.3 spaces per patron permitted	39 ⁽²⁾	0	-39 ⁽²⁾
TOTAL			138	35	-103

Notes:

1. Clause 52.06-5 specifies that where a car parking calculation results in a requirement that is not a whole number, then number of spaces should be rounded down to the nearest whole number.
2. Parking requirement and shortfall does not factor in the existing 80 patron parish hall. If accounted for the place of assembly requirement would be 15 spaces and shortfall -15

The provision of 35 car spaces results in a statutory shortfall of 103 spaces (11 office, 34 shop, 19 food and drink premises and 39 place of assembly spaces) under Clause 52.06-5 and accordingly, a car parking reduction is required under Clause 52.06-7.

Disabled Parking

Clause 52.06-9 states that:

The car parking requirement specified in Table 1 includes disabled car parking spaces. The proportion of spaces to be allocated as disabled spaces must be in accordance with Australian Standard AS2890.6-2009 (disabled) and the Building Code of Australia.

One disabled car space is required under the NCC in relation to the commercial car parking. One disabled car space is provided on-site, and accordingly, this requirement is met.

4.1.1. Reducing the Requirement for Car Parking

Clause 52.06-7 allows for the statutory car parking requirement to be reduced (including to zero). An application to reduce (including reduce to zero) the number of car spaces required under Clause 52.06-5 or in a schedule to the Parking Overlay must be accompanied by a Car Parking Demand Assessment.

Clause 52.06-7 sets out that a Car Parking Demand Assessment must have regard to the following key factors:

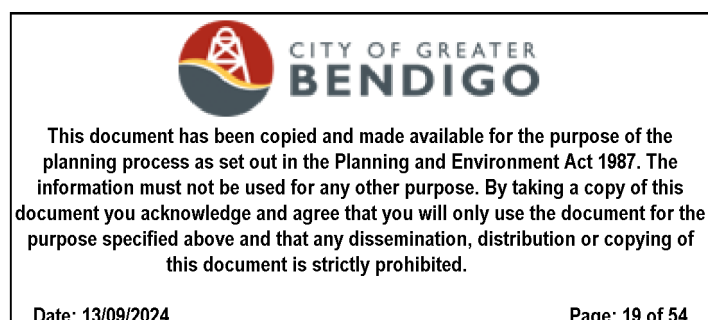
- *The likelihood of multi-purpose trips within the locality which are likely to be combined with a trip to the land in connection with the proposed use.*
- *The variation of car parking demand likely to be generated by the proposed use over time.*
- *The short-stay and long-stay car parking demand likely to be generated by the proposed use.*
- *The availability of public transport in the locality of the land.*
- *The convenience of pedestrian and cyclist access to the land.*
- *The provision of bicycle parking and end of trip facilities for cyclists in the locality of the land.*
- *The anticipated car ownership rates of likely or proposed visitors to or proposed occupants (residents or employees) of the land.*
- *Any empirical assessment or case study.*

Planning Practice Note 22 (August, 2022) specifies that the provisions for reducing the car parking requirement draw a distinction between the assessment of likely demand for car parking spaces (the Car Parking Demand Assessment), and whether it is appropriate to allow the supply of fewer spaces than assessed by the Car Parking Demand Assessment. These are two separate considerations, one technical while the other is more strategic. Different factors are taken into account in each consideration.

Accordingly, the applicant must satisfy the responsible authority that the provision of car parking is appropriate on the basis of a two-step process, which has regard to:

- *The car parking demand likely to be generated by the use.*
- *Whether it is appropriate to allow fewer spaces to be provided than the number likely to be generated by the site.*

An assessment of the appropriateness of reducing the car parking provision below the statutory requirement is set out below.



4.1.2. Car Parking Demand Assessment

Office

We consider that the statutory car parking rate of 2.5 car spaces per 100m² for the office tenancies is generally representative of the expected car parking demands given the locale of the site.

Accordingly, through the provision of 33 office spaces, it is expected that there will be a shortfall of 11 office staff spaces that will need to be accommodated in the nearby area. These demands will be long-term in nature.

Chocolaterie

We consider that the combined statutory requirements for the shop and food and drink premises is generally representative of the expected car parking demands for the Chocolaterie. For simplicity, we have also included the floor area of the small additional retail tenancy within the overall Chocolaterie car parking demands. Accordingly, the Chocolaterie is expected to generate a demand for up to 55 car spaces.

Given that two car parking spaces are allocated to the Chocolaterie, we expect that these parking demands will generally need to be met in the nearby area (i.e. there will be a shortfall of 53 spaces). The parking demands will be a mixture of short-term customer demands, and long-term staff demands.

Approximately a third, or 18 spaces will be staff demands, with the remaining 35 being customer parking demands.

In practice, the Chocolaterie is expected to be a tourist destination, that will attract tourists who may walk to the site, or seek alternative modes of transport (as outlined in Section 3.4).

Visitors will include many tourists visiting Bendigo and locals who will walk or ride around the CBD. i.e. tourists who will be parking elsewhere (or coming by train from Melbourne) and then walking between key attractions in town (Bendigo Art Gallery, View Street precinct, Rosalind Park, Visitor Information Centre, Gold Mine, etc.).

It is also expected that a portion of customers will travel via tourist coaches, which will require significantly less car parking than if travelling separately via private vehicle.

Bendigo Heritage Attraction has a strategic plan to install a new tram stop outside the Cathedral, enabling people to visit the site more easily.

We understand that the Chocolaterie website will provide information on their website regarding the alternative options and ways to access the site.

Accordingly, it is expected that customer demands will be some amount less than indicated above.

Additionally, the above assessment is conservative, as it does not allow for staff of the 'Chocolaterie' parking on-site outside of the office operating periods (i.e. on weekends). Accordingly, during these times the staff demands are expected to be met on-site.

Place of Assembly

Given there is an existing parish on the site which accommodate 80 patrons, the proposal represents an increase of 50 patrons which will practically result in demand for a further 15

car spaces. Given no car parking is provided on-site this demand will need to be met in the nearby area.

It is important to note that the peak times for the place of assembly is unlikely to correspond with that of the office or Chocolaterie.

4.1.3. Appropriateness of Providing Fewer Car Spaces than the Demand Assessment

If the number of car spaces is not met on-site under the Car Parking Demand Assessment, the second step is to consider whether it is appropriate to allow fewer spaces to be provided than the number likely to be generated by the site as assessed by the Car Parking Demand Assessment.

Clause 52.06-7 sets out a series of car parking provision factors that should be considered when assessing the appropriateness of providing fewer car spaces on the site than are likely to be generated by the use.

The most relevant factors to this application are considered below.

Table 5: Review of Car Parking Reduction Factors

Decision factor of Clause 52.06-7	Response
<p><i>Any relevant local planning policy or incorporated plan.</i></p>	<p>The proposed car parking reduction is in line with local Council Transport Policy that prioritises alternative modes of transport over private vehicle usage, including Clause 18 (Land Use and Transport) of the Planning Scheme and Greater Bendigo’s Integrated Transport and Land Use Strategy (ITLUS, dated 26th August, 2015).</p>
<p><i>The availability of alternative car parking in the locality of the land, including:</i></p> <ul style="list-style-type: none"> • <i>Efficiencies gained from the consolidation of shared car parking spaces.</i> • <i>Public car parks intended to serve the land.</i> • <i>On street parking in non residential zones.</i> • <i>Streets in residential zones specifically managed for non-residential parking.</i> 	<p>The detailed car parking surveys were undertaken by our office are presented at Section 3.3.</p> <p>These surveys determined that there was a minimum of 136 vacant spaces were recorded over the survey period (76% occupancy), which occurred at 1pm on Friday 5th May, 2023. Accordingly, we are satisfied that there is adequate on-street car parking in the nearby area to accommodate the expected overflow in car parking.</p> <p>Furthermore, there will be 72 car spaces provided across the precinct’s combined frontages post-development, which can largely accommodate the expected overflow in parking demands.</p> <p>Parking demands for spaces along the site’s frontage are generally low to moderate. Additionally, 40 of these spaces are unrestricted,</p>

Decision factor of Clause 52.06-7	Response
<div data-bbox="38 600 750 918" style="border: 1px solid black; padding: 5px;">  <p>This document has been copied and made available for the purpose of the planning process as set out in the Planning and Environment Act 1987. The information must not be used for any other purpose. By taking a copy of this document you acknowledge and agree that you will only use the document for the purpose specified above and that any dissemination, distribution or copying of this document is strictly prohibited.</p> <p>Date: 13/09/2024 Page: 22 of 54</p> </div> <p>Any adverse economic impact a shortfall of parking may have on the economic viability of any nearby activity centre.</p> <p>The future growth and development of any nearby activity centre.</p>	<p>and suitable for long-term parking by staff, while the remainder is typically short-term parking by customers.</p> <p>In the overall area, there are 128 unrestricted spaces that front non-residential properties, which were typically in moderate demand. There were an average of 40 vacant spaces within these areas over the survey period, which are suitable for staff of the development.</p> <p>There is also further short-term parking available along High Street, which was typically in low to moderate demand.</p> <p>In addition to on-street car parking, there are a number of off-street multi-storey car parks through Bendigo, the closest of which is located approximately 500m south-east of the site, and provides 420 car spaces, which can be utilised for both short-term and long-term car parking. Of these 420 spaces, 399 spaces are publicly available for long-term use and also suitable for use by staff of the development.</p> <p>Based on the above, we are satisfied that there is adequate car parking availability in the nearby area to accommodate the expected overflow in parking demands associated with the proposed development.</p>
	<p>The site is located within the Bendigo CBD. Planning Practice Note 22 (August, 2022) states that:</p> <p><i>In an Activity Centre, car parking issues have a part to play, but should not dominate when assessing an application for a use or development.</i></p> <p><i>Where a change of use or relatively small extension is consistent with the strategic plan for the centre and car parking cannot easily be provided, it will often be more sensible to reduce the car parking requirement, rather than prevent the use or development. Some activity centres will have excellent public transport access, ample car parking or mainly serve local customers who arrive on foot. In such circumstances, an increase in business and activity would increase the</i></p>

Decision factor of Clause 52.06-7	Response
	<p><i>overall viability of the centre, and the reduced number of car trips would have a positive impact.</i></p> <p>We are satisfied that the proposed development is located in an area that is well served by public transport, will add to the overall viability of the centre, and that the reduced number of car trips would have a positive impact on the nearby area.</p>
<p><i>Local traffic management in the locality of the land.</i></p>	<p>An 'office' use is one land-use that is particularly conducive (and important to target) in achieving a mode shift away from private cars to public transport, cycling, walking, etc. This also applies to staff parking for most commercial businesses.</p> <p>This is particularly the case as journey to work trips for office uses are typically made during the commuter peak hours and predominantly involve single occupant vehicles. The timing of these trips has the greatest impact on traffic congestion on the road network and occurs when public transport services operate at high frequencies (and offer express services in some cases).</p> <p>The reduced provision of car parking assists in reducing the traffic impacts of the development on the local and broader road network and encourages sustainable transport choices. If provided with the full statutory requirement for car parking, traffic generation by the development would be higher than what is proposed.</p>
<p><i>Access to or provision of alternative transport modes to and from the land.</i></p>	<p>The alternative modes of transport in close proximity to the site are detailed in Section 3.4. The site is located in close proximity to effectively every bus service in Bendigo. Additionally, the site is also within walking distance of Bendigo Railway Station.</p> <p>The site also provides bicycle parking and end of trip facilities in excess of the minimum statutory requirements (see Section 4.2).</p> <p>We are satisfied that the alternative modes of transport in the surrounding area supports the car parking reduction sought.</p>
<p><i>Any other matter specified in a schedule to the Parking Overlay.</i></p>	<p>The Parking Overlay considerations are discussed in Section 4.1.4.</p>



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4.1.4. Review of Decision Guidelines of Schedule 1 to Clause 45.09

In addition to the decision guidelines of Clause 52.06-7, Clause 45.09-4 also provides additional considerations when applying for a reduction required in the number of car parking spaces.

These are reviewed in the following table.

Table 6: Review of Clause 45.09 Decision Guidelines

Decision Factor	Response
<i>Whether historic contributions have been made towards the provision of car parking facilities.</i>	We understand that no historic contributions have been made in relation to the subject site.
<i>Whether the constraints of the site would severely limit opportunities to provide on-site car parking facilities.</i>	The proposed building design is built into the sloping topography, draws attention to the heritage-listed Cathedral, creates new landscaped public open spaces for community activities and meeting places. The development includes 35 on-site car parking spaces in the on-site carpark, without adversely affecting the aesthetic of the precinct.
<i>Whether the overall benefits of the development would outweigh the need to provide the full number of on-site parking spaces and make a more efficient use of the land.</i>	The overall benefits of the development is not a traffic engineering issue that we can advise on (i.e. it is outside our area of expertise).
<i>Whether a range of sustainable transport initiatives have been adopted including, but not limited to:</i> <ul style="list-style-type: none"> <i>Full provision of bicycle and 'end of trip' facilities as set out under Clause 52.34; and</i> <i>Preparation of a green travel plan which includes a variety of transport demand management measures that reduce peoples' dependency on private vehicle trips.</i> 	The bicycle parking provision is discussed in Section 4.2. Bicycle parking and end of trip facilities are provided in excess of the minimum requirements under Clause 52.34. Accordingly, we are satisfied that the bicycle parking provision supports the car parking reduction sought.
<i>Whether a better urban design or heritage outcome would be achieved through the provision of parking in an off-street public parking facility or via a financial contribution.</i>	This is a heritage and urban design matter, and not specifically relate to traffic engineering.
<i>In relation to residential development, the responsible authority may reduce the requirements of Table above upon consideration of the following:</i> <ul style="list-style-type: none"> <i>Whether or not the proposal is a new construction or if it is the reuse of an existing</i> 	The development is not residential.

Decision Factor	Response
<p><i>building (generally new buildings will be required to provide parking on-site); and</i></p> <ul style="list-style-type: none"> <i>If a range of sustainable transport initiatives can be incorporated into the development.</i> 	

Clause 45.09-5 also states the following:

The responsible authority may consider accepting a financial contribution-in-lieu of one or more car parking spaces being provided on-site.

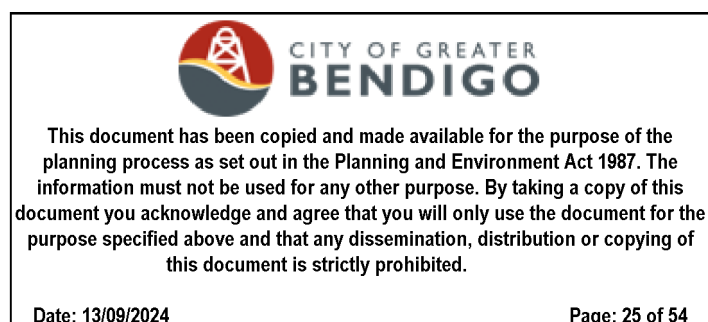
When car parking spaces are to be provided off-site the cash-in-lieu contribution per car space is \$10,561.67 per space (no GST) for each car space. The amount of contribution for each car space specified above will be adjusted by the responsible authority on 1 July each year in accordance with the relevant Building Price Index (Melbourne) in Rawlinsons Australian Construction Handbook. If that index is unavailable, an equivalent index will be applied by the responsible authority.

The *Bendigo CBD Parking Strategy, 2008* specifies that the financial contributions will be utilised to increase the supply of parking through the construction of multi-deck car parks in the locale.

This plan lists the multi-storey carpark at the corner of Edward Street and Queen Street, which has since been constructed. The existing Edward Street carpark is approximately 500m from the site.

Given the distance of this carpark from the site, we consider that customers of the Chocolaterie (i.e. the greatest portion of the overflow car parking), will likely park in closer proximity to the site, including along the precincts' 4 frontages.

Accordingly, we are satisfied that the decision guidelines of the Planning Scheme support the car parking reduction sought, and that a financial contribution is not necessary in this context.



4.2. Bicycle Parking Provision

Clause 52.34 of the Planning Scheme specifies bicycle parking requirements for new developments. The purpose of Clause 52.34 is to:

- To encourage cycling as a mode of transport.
- To provide secure, accessible and convenient bicycle parking spaces and associated shower and change facilities.

The development provides bicycle parking as follows:

- 15 secure bicycle spaces within the carpark, provided as follows:
 - 5 'Ned Kelly' bicycle racks
 - 10 ground level hoops
- 14 bicycle spaces via open rails at ground level next to the cafe.
- 10 bicycle spaces via open rails at street level to the south-west of the proposed building.

Ten additional bicycle spaces will also be provided at ground level on the adjacent Cathedral property (but still within the overall precinct).

The statutory bicycle parking requirement of the development under Clause 52.34 is set out in the table below.

Table 7: Statutory Bicycle Parking Assessment - Clause 52.34

Use	Size/No.	Statutory Bicycle Parking Requirement		No. Bicycle spaces required
		Employees	Visitors or Customers	
Office	1,777m ²	1 space to each 300m ² NFA if NFA exceeds 1,000m ²	1 space to each 1,000m ² NFA if NFA exceeds 1,000m ²	6 employees 2 visitor
Shop	1,050m ²	1 space to each 600m ² LFA if the LFA exceeds 1000m ²	1 space to each 500m ² LFA if the LFA exceeds 1000m ²	2 staff 2 customer
Retail (Food & Drink Premises)	566m ²	1 space to each 300m ² LFA	1 space to each 500m ² LFA	2 staff 1 customer
Place of Assembly	148m ²	1 space to each 1,500m ² NFA	2, plus 1 to each 1,500m ² NFA	0 staff 2 visitor
TOTAL				17 spaces


Based on the above, the provision of 39 on-site bicycle spaces (plus 10 adjacent to the site) exceeds the bicycle parking provision requirements of Clause 52.34.

Clause 52.34 also requires consideration of end-of-trip facilities and the design of the bicycle parking spaces. The table below reviews the design and provision of these facilities.

Table 8: Design of Bicycle Parking

Requirement	Assessment	Design Response
End of Trip Facilities - Table 2 & 3 of Clause 52.34-5		
If 5 or more employee bicycle spaces are required, 1 one shower for the first 5 employee bicycle spaces, plus 1 to each 10 employee bicycle spaces thereafter.	✓	Two showers are required for 10 employee bicycle spaces and three are provided, including one accessible WC and shower.
1 change room or direct access to a communal change room to each shower. The change room may be a combined shower and change room.	✓	The changerooms are combined with the showers.
Design of Bicycle Parking		
Does the design comply with the design requirements of Clause 52.34-6?	✓	All bicycle spaces are designed in accordance with the bicycle parking specifications of Clause 52.34 and AS2890.3-2015.
Does the design comply with the requirements of AS2890.3-2015?	✓	

Based on the above, we are satisfied that the provision of bicycle parking and end of trip facilities accord with the requirements of Clause 52.34.



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4.3. Review of Carpark Layout and Vehicle Access Arrangements


Traffic Group has provided design advice to the project architect to achieve a satisfactory carpark layout. The proposed parking layout has been assessed under the following guidelines:

- Clause 52.06-9 of the Planning Scheme (Design Standards for car parking),
- AS2890.1-2004 – Part 1: Off-Street Car Parking (where relevant), and
- AS2890.6-2009 – Part 6: Off-Street Car Parking for People with Disabilities.

An assessment against the relevant design standards of the Planning Scheme and Australian Standards (where relevant) is provided in the table below.

Table 9: Carpark Layout and Access Assessment

Requirement	Assessment	Design Response
Clause 52.06-9 Design Standard 1 – Accessways		
Must be at least 3m wide	✓	Accessways are greater than 3m in width.
Have an internal radius of at least 4m at changes of direction or intersection or be at least 4.2m wide.	✓	B99 design car can navigate all bends. Objective achieved.
Allow vehicles parked in the last space of a dead-end accessway in public car parks to exit in a forwards direction with one manoeuvre.	N/A	Carpark will not be publicly accessible.
Provide at least 2.1m headroom beneath overhead obstructions, calculated for a vehicle with a wheel base of 2.8m.	✓	Minimum headroom of 2.2m provided throughout carpark.
If the accessway serves four or more car spaces or connects to a road in a Transport Zone 2 or Transport Zone 3, the accessway must be designed so that cars can exit the site in a forward direction.	✓	All cars can exit the site in a forward direction.



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Requirement	Assessment	Design Response
Provide a passing area at the entrance at least 6.1m wide and 7m long if the accessway serves ten or more car parking spaces and is either more than 50m long or connects to a road in a Transport Zone 2 or Transport Zone 3.	✓	A passing area at the entrance to the site (measuring 6.1m wide by 7m long) is not required as whilst the accessway serves more than 10 car spaces, the length of accessway which is single-width is far less than 50m and does not connect to a Transport Zone. Additionally, following the entry ramp into the site, two-way passing is possible throughout the internal carpark.
Have a corner splay or area at least 50% clear of visual obstructions extending at least 2m along the frontage road from the edge of an exit lane and 2.5m along the exit lane from the frontage, to provide a clear view of pedestrians on the footpath of the frontage road. The area clear of visual obstructions may include an adjacent entry or exit lane where more than one lane is provided, or adjacent landscaped areas, provided the landscaping in those areas is less than 900mm in height.	✓	Sight triangle is available along the access point on approach to the footpath on both sides of the accessway.
If an accessway to four or more car parking spaces is from land in a Transport Zone 2 or Transport Zone 3, the access to the car spaces must be at least 6m from the road carriageway.	N/A	No accessway is proposed to a Transport Zone.
If entry to the car space is from a road, the width of the accessway may include the road.	N/A	No car parking is provided directly to the road.



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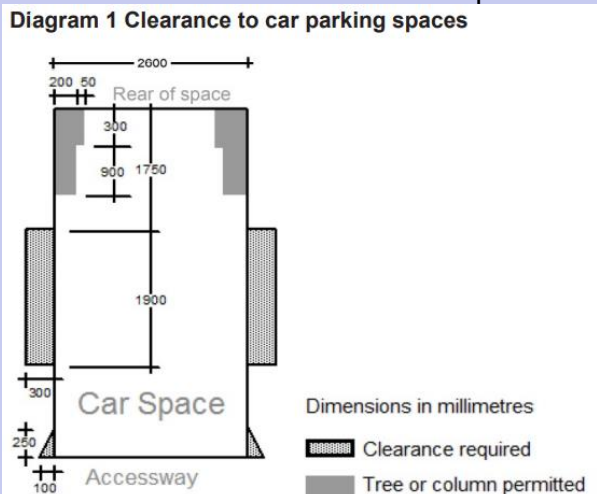
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Requirement	Assessment	Design Response																														
Clause 52.06-9 Design Standard 2 – Car Parking Spaces																																
<p>Car parking spaces and accessways must have the minimum dimensions as outlined in Table 2 under Clause 52.06-9.</p> <table border="1"> <thead> <tr> <th>Angle of car spaces to accessway</th> <th>Accessway width</th> <th>Car park width</th> <th>Car park length</th> </tr> </thead> <tbody> <tr> <td>Parallel</td> <td>3.6 m</td> <td>2.3 m</td> <td>6.7 m</td> </tr> <tr> <td>45°</td> <td>3.5 m</td> <td>2.6 m</td> <td>4.9 m</td> </tr> <tr> <td>60°</td> <td>4.9 m</td> <td>2.6 m</td> <td>4.9 m</td> </tr> <tr> <td rowspan="3">90°</td> <td>6.4 m</td> <td>2.6 m</td> <td>4.9 m</td> </tr> <tr> <td>5.8 m</td> <td>2.8 m</td> <td>4.9 m</td> </tr> <tr> <td>5.2 m</td> <td>3.0 m</td> <td>4.9 m</td> </tr> <tr> <td></td> <td>4.8 m</td> <td>3.2 m</td> <td>4.9 m</td> </tr> </tbody> </table> <p><i>Note to Table 2: Some dimensions in Table 2 vary from those shown in the Australian Standard AS2890.1-2004 (off street). The dimensions shown in Table 2 allocate more space to aisle widths and less to marked spaces to provide improved operation and access. The dimensions in Table 2 are to be used in preference to the Australian Standard AS2890.1-2004 (off street) except for disabled spaces which must achieve Australian Standard AS2890.6-2009 (disabled).</i></p>	Angle of car spaces to accessway	Accessway width	Car park width	Car park length	Parallel	3.6 m	2.3 m	6.7 m	45°	3.5 m	2.6 m	4.9 m	60°	4.9 m	2.6 m	4.9 m	90°	6.4 m	2.6 m	4.9 m	5.8 m	2.8 m	4.9 m	5.2 m	3.0 m	4.9 m		4.8 m	3.2 m	4.9 m	✓	<p>All car spaces are 2.6m wide x 4.9m with a 6.4m wide access aisle.</p> <p>Access to and from the critical car spaces within the basement carpark have been checked for access by the B85 design car (specified at Appendix B of AS2890.1-2004).</p>
Angle of car spaces to accessway	Accessway width	Car park width	Car park length																													
Parallel	3.6 m	2.3 m	6.7 m																													
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	5.8 m	2.8 m	4.9 m																													
	5.2 m	3.0 m	4.9 m																													
	4.8 m	3.2 m	4.9 m																													
<p>A wall, fence, column, tree, tree guard or any other structure that abuts a car space must not encroach into the area marked 'clearance required' on Diagram 1, other than:</p> <ul style="list-style-type: none"> A column, tree or tree guard, which may project into a space if it is within the area marked 'tree or column permitted' on Diagram 1. A structure, which may project into the space if it is at least 2.1 metres above the space. <p>Diagram 1 Clearance to car parking spaces</p> 	✓	<p>All column and other obstructions are located as per Diagram 1 of Clause 52.06.</p>																														
<p>Car spaces in garages/carports must be at least 6m long and 3.5m wide for a single space and 5.5m wide for a double space measured inside the garage/carport.</p>	N/A	No garages proposed.																														

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Requirement	Assessment	Design Response
Where parking spaces are provided in tandem, an additional 0.5m in length must be provided between each space.	N/A	No tandem car spaces.
Where two or more car parking spaces are provided for a dwelling, at least one space must be under cover.	✓	All spaces are under cover.
Disabled car parking spaces must be designed in accordance with AS2890.6-2009 and the Building Code of Australia. Disabled car parking spaces may encroach into an accessway width specified in Table 2 by 0.5m. A minimum headroom of 2.5m is to be provided above the disabled car space in accordance with AS2890.6-2009.	✓	Disabled parking complies with the requirements of AS2890.6-2022.

Clause 52.06-9 Design Standard 3 - Gradients

Accessway grades must not be steeper than 1:10 (10 per cent) within 5 metres of the frontage to ensure safety for pedestrians and vehicles. The design must have regard to the wheelbase of the vehicle being designed for; pedestrian and vehicular traffic volumes; the nature of the car park; and the slope and configuration of the vehicle crossover at the site frontage. This does not apply to accessways serving three dwellings or less.	✓	Accessway does not exceed 1:10 anywhere in the carpark.													
Ramps (except within 5 metres of the frontage) must have the maximum grades as outlined in Table 3 and be designed for vehicles travelling in a forward direction.	✓	Internal carpark is all level.													
<table border="1"> <thead> <tr> <th>Type of car park</th> <th>Length of ramp</th> <th>Maximum grade</th> </tr> </thead> <tbody> <tr> <td rowspan="2">Public car parks</td> <td>20 metres or less</td> <td>1:5 (20%)</td> </tr> <tr> <td>longer than 20 metres</td> <td>1:6 (16.7%)</td> </tr> <tr> <td rowspan="2">Private or residential car parks</td> <td>20 metres or less</td> <td>1:4 (25%)</td> </tr> <tr> <td>longer than 20 metres</td> <td>1:5 (20%)</td> </tr> </tbody> </table>	Type of car park	Length of ramp	Maximum grade	Public car parks	20 metres or less	1:5 (20%)	longer than 20 metres	1:6 (16.7%)	Private or residential car parks	20 metres or less	1:4 (25%)	longer than 20 metres	1:5 (20%)		
Type of car park	Length of ramp	Maximum grade													
Public car parks	20 metres or less	1:5 (20%)													
	longer than 20 metres	1:6 (16.7%)													
Private or residential car parks	20 metres or less	1:4 (25%)													
	longer than 20 metres	1:5 (20%)													
Where the difference in grade between two sections of ramp or floor is greater than 1:8 (12.5 per cent) for a summit grade change, or greater than 1:6.7 (15 per cent) for a sag grade change, the ramp must include a transition section of at least 2 metres to prevent vehicles scraping or bottoming.	N/A	Internal carpark is all level.													
Plans must include an assessment of grade changes of greater than 1:5.6 (18 per cent) or less than 3 metres apart for clearances, to the satisfaction of the responsible authority	N/A	Internal carpark is all level.													

Requirement	Assessment	Design Response
Clause 52.06-9 Design Standard 4 – Mechanical Parking		
At least 25 per cent of the mechanical car parking spaces can accommodate a vehicle height of at least 1.8 metres.	N/A	No mechanical parking proposed.
Car parking spaces that require the operation of the system are not allocated to visitors unless used in a valet parking situation.	N/A	
The design and operation is to the satisfaction of the responsible authority.	N/A	
Clause 52.06-9 Design Standard 5 – Urban Design		
Ground level car parking, garage doors and accessways must not visually dominate public space.	N/A	These matters are more related to urban design, rather than specifically traffic engineering, however the design complies with these requirements.
Car parking within buildings (including visible portions of partly submerged basements) must be screened or obscured where possible, including through the use of occupied tenancies, landscaping, architectural treatments and artworks.		
Design of car parks must take into account their use as entry points to the site.		
Design of new internal streets in developments must maximise on street parking opportunities.	N/A	No internal streets proposed
Clause 52.06-9 Design Standard 6 – Safety		
Car parking must be well lit and clearly signed.	✓	Car parking is all private for use by staff, and we are satisfied that signage is not strictly required. The carpark will be lit during operating hours of each premises.
The design of car parks must maximise natural surveillance and pedestrian visibility from adjacent buildings.	✓	We are satisfied that the common accessway naturally provides good sightlines.

Requirement	Assessment	Design Response
Pedestrian access to car parking areas from the street must be convenient.	✓	Separate pedestrian accesses are provided to both Short Street and High Street. We are satisfied that these arrangements represent convenient pedestrian access.
Pedestrian routes through car parking areas and building entries and other destination points must be clearly marked and separated from traffic in high activity parking areas.	✓	Given that the carpark consists of only 35 car spaces that are all allocated to staff, traffic levels within the carpark are expected to be low. In any event, accessways are at least 6.1m, which can readily accommodate shared pedestrian and vehicle movements. We are satisfied that these arrangements are appropriate.
Clause 52.06-9 Design Standard 7 - Landscaping		
The layout of car parking areas must provide for water sensitive urban design treatment and landscaping.	N/A	Not applicable to internal carparks.
Landscaping and trees must be planted to provide shade and shelter, soften the appearance of ground level car parking and aid in the clear identification of pedestrian paths.		
Ground level car parking spaces must include trees planted with flush grilles. Spacing of trees must be determined having regard to the expected size of the selected species at maturity.		



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4.4. Loading and Waste Collection Arrangements

Clause 65.01 of the Planning Scheme states that the Responsible Authority must consider a number of matters as appropriate including:

- *The adequacy of loading and unloading facilities and any associated amenity, traffic flow and road safety impacts.*

4.4.1. Loading

The proposal includes an on-site loading bay within the carpark, which will be accessed via Short Street.

The loading bay will be 4.655m wide x 12.5m long and can accommodate vehicles up to the 6.4m long Small Rigid Vehicle (SRV), which will service the Chocolaterie and the offices. The headroom within this area will be 3.5m and grades leading from Short Street are in accordance with the requirements of AS2890.2-2018.

Swept path diagrams demonstrating access to this area are attached at Appendix C.

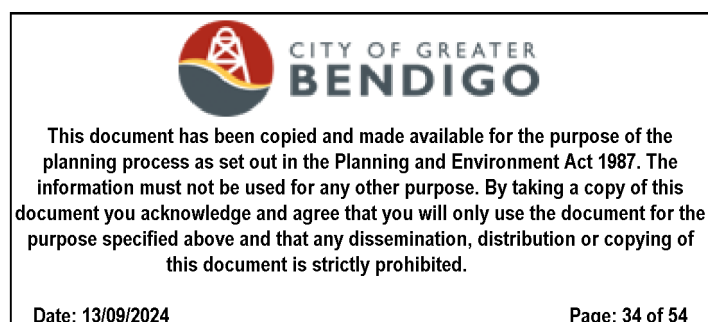
We are satisfied that the proposed loading arrangements are acceptable.

4.4.2. Waste Collection

A Waste Management Plan has been prepared by our office (Report Ref: G31621R-02F, dated March, 2024), which sets out that waste collection is to occur along the site's frontage to Short Street. Waste collection will be undertaken by private contractor for the 'Chocolaterie' and via Council's services for the offices. The site operator will be responsible for transferring the bins between the internal bin enclosure and Short Street kerb on collection day.

It is important to note that the building design has provision for 6.4m long mini waste trucks to unload bins in the loading dock area, however these trucks are not currently available in the Bendigo area. In the future, contractors may operate with these trucks in the area, at which point waste collection arrangements could be reviewed.

Accordingly, we satisfied that the waste collection arrangements are acceptable.



4.5. Traffic Impact Assessment

On-Site Carpark

Given the reduced rate of car parking for the proposed development, we consider that a first-principles approach is most appropriate in this instance.

Surveys undertaken by our office and other consultants have determined that office car spaces typically generate 0.5 trips per peak hour period.

Based on the provision of 35 car spaces, we expect that the site will generate in the order of 18 vehicle trips during each peak hour period.

This level of traffic is low and represents 1 vehicle trip every 3-4 minutes.

We are satisfied that this level of traffic can be accommodated by the surrounding road network without any adverse impact to its operation.

Off-Site Parking

The traffic impacts as a result of off-site car parking will be mostly related to customers travelling to and from the Chocolaterie, (in addition to a small number of staff). The staff trips will be minimal, and will be generally limited to the start and end of shift times.

Customers will travel to the site, and will be spread through the on-street car parking areas in the nearby area. Additionally, many customers will visit a number of sites in the CBD and will park accordingly, such as in other streets, their accommodation, or multi-storey car parks. Other visitors will be dropped off by tour or school coaches.

We are satisfied that the customer traffic impacts will be spread through the nearby area, and can be readily accommodated by the surrounding road network.



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5. Conclusions

Having undertaken a detailed traffic engineering assessment of the Cathedral Precinct Redevelopment, Bendigo, we are of the opinion that:

- a) the proposed development has a statutory car parking requirement of 138 car spaces under Clause 52.06-5, including 44 office, 36 shop, 19 food and drink premises and 39 place of assembly car spaces,
- b) the provision of 33 office spaces and 2 staff spaces for the shop results in a statutory shortfall of 103 car spaces, comprising 11 office, 34 shop, 19 food and drink premises and 39 place of assembly spaces, and accordingly, a car parking reduction is required under Clause 52.06-7,
- c) the car parking shortfall is considered appropriate due to:
 - i) the availability of car parking in the nearby area, including along the site's frontage,
 - ii) the impact of the development on the surrounding activity centre,
 - iii) the access to alternative modes of transport nearby to the site,
 - iv) the reduced level of car parking is consistent with Council's local policies,
 - v) the car parking reduction is consistent with the decision guidelines of Schedule 1 to Clause 45.09, and
 - vi) the shortfall in place of assembly car parking practically represents a continuation of the existing conditions.
- d) the proposed parking layout and vehicle access arrangements accord with the requirements of the Planning Scheme, Australian Standards (where relevant) and current practice,
- e) bicycle parking is provided in excess of the Clause 52.34 requirements of the Planning Scheme and accords with the design requirements of AS2890.3-2015,
- f) the level of traffic generated by the proposal can be accommodated without any adverse impacts to the operation of the local road network,
- g) the proposed loading bay on-site is acceptable, and will accommodate the expected loading activities of the development,
- h) waste collection will be undertaken via the Short Street kerbside, and
- i) there are no traffic engineering reasons why a planning permit for the Cathedral Precinct Redevelopment, Bendigo should be refused, subject to appropriate conditions.



CITY OF GREATER
BENDIGO

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Appendix A

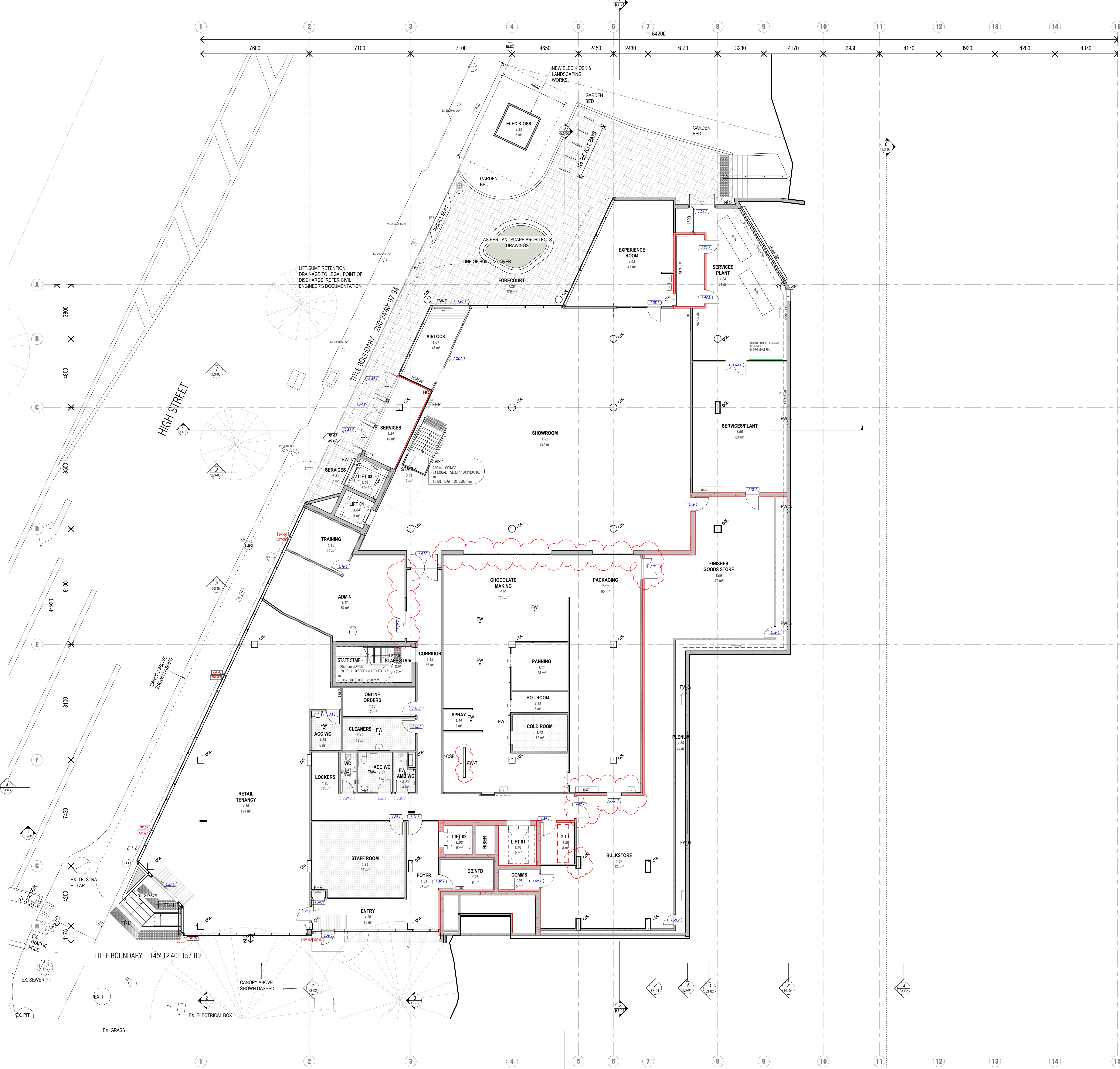
Proposed Development Plans



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KEYNOTE LEGEND	
Code	Description
CSB	
OP-02	
FWR	Floor Waste
FW-G	
FW-T	
HC	
CF-01	
TT-01	Tactile Indicators Internal (Refer Landscape for external facilities)
TT-11	

03 - Internal Net Floor Area - Room			
Number	Name	Department	Area
T.01	AIRLOCK	Chocolate	15 m ²
T.02	SHOWROOM	Chocolate	327 m ²
T.03	EXPERIENCE ROOM	Chocolate	45 m ²
T.04	SERVICES PLANT	Plant	67 m ²
T.05	SERVICES PLANT	Chocolate	63 m ²
T.06	FRESHES GOODS STORE	Chocolate	97 m ²
T.07	BULKSTORE	Chocolate	82 m ²
T.08	COMMS	Chocolate	3 m ²
T.09	CHOCOLATE MAKING	Chocolate	114 m ²
T.10	PACKAGING	Chocolate	85 m ²
T.11	PACKAGING	Chocolate	13 m ²
T.12	HOT ROOM	Chocolate	8 m ²
T.13	COLD ROOM	Chocolate	11 m ²
T.14	SPRAY	Chocolate	3 m ²
T.15	CORRIDOR	Chocolate	66 m ²
T.16	TRAINING	Chocolate	15 m ²
T.17	ADMIN	Chocolate	50 m ²
T.18	ONLINE ORDERS	Chocolate	12 m ²
T.19	CLEANERS	Chocolate	12 m ²
T.20	LOCKERS	Chocolate	10 m ²
T.21	WC	Chocolate	4 m ²
T.22	ACC WC	Chocolate	7 m ²
T.23	AMB WC	Chocolate	4 m ²
T.24	STAFF ROOM	Chocolate	35 m ²
T.25	DBNTD	Chocolate	9 m ²
T.26	ENTRY	Chocolate	15 m ²
T.28	ACC WC	Shand	3 m ²
T.30	PLENUM	Chocolate	38 m ²
T.31	POWER	Shand	18 m ²
T.32	ELEC KIOSK	Shand	8 m ²
T.33	SERVICES	Shand	7 m ²
T.34	SERVICES	Plant	18 m ²
T.35	G.I.1	Chocolate	8 m ²
T.36	RETAIL TENANCY	Other	199 m ²
L.01	LIFT 01	Chocolate	9 m ²
L.02	LIFT 02	Shand	5 m ²
L.03	LIFT 03	Shand	6 m ²
L.04	LIFT 04	Chocolate	6 m ²
S.01	STAIR 1	Chocolate	7 m ²
S.03	STAFF STAIR	Chocolate	17 m ²
S.04	FIRE STAIR	Shand	16 m ²
S.04	FIRE STAIR	Shand	8 m ²
Exp. 43			1533 m ²

2024.08.23	WIP ISSUE	C9
2024.08.21	ISSUE FOR INFORMATION	C8
2024.08.16	WIP ISSUE	C7
2024.08.08	LEASE PLAN UPDATE	C7
2024.08.02	WIP ISSUE	C5
2024.07.26	ISSUE FOR COORDINATION	C4
2024.07.25	DRAFT ISSUE	C3
2024.07.22	ISSUE FOR COORDINATION	C2
Date	Description	Issue

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 NOMINATED ARCHITECT:
 ANDREW MASTERS

BENDIGO SACRED HEART CATHEDRAL SQUARE

Cathedral Square General Arrangement - Level 1 (Ground)

Drawn	Approved	Date	Scale
AF/AA/AJ	SIB	2024.08.23	1:100 @ A0

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LEVEL 1 FLOOR PLAN
1:100

SHORT STREET

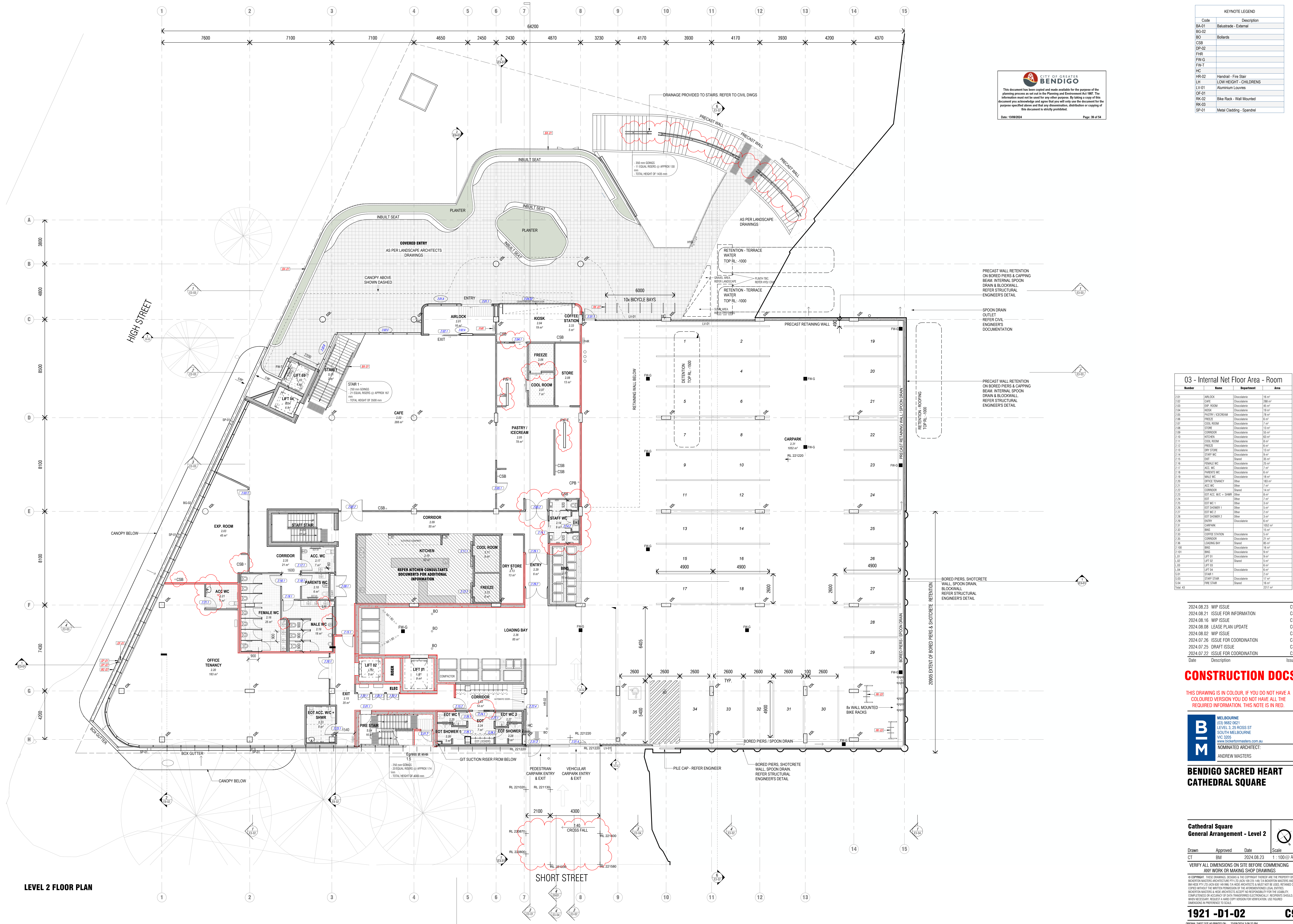
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Code	Description
BA-01	Balustrade - External
BIG-02	
BO	Bolards
CSB	
DP-02	
FHR	
FW-G	
FW-T	
HC	
HR-02	Handrail - Fire Star
LN	LOW HEIGHT - CHILDRENS
LV-01	Aluminum Louvers
OF-01	
RK-02	Bike Rack - Wall Mounted
RK-03	
SP-01	Metal Cladding - Spandrel

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03 - Internal Net Floor Area - Room

Number	Name	Department	Area
2.01	AIRLOCK	Chocovina	16 m ²
2.02	CARP	Chocovina	288 m ²
2.03	TOP ROOM	Chocovina	42 m ²
2.04	KIOSK	Chocovina	19 m ²
2.05	PASTRY / ICECREAM	Chocovina	18 m ²
2.06	FREEZE	Chocovina	8 m ²
2.07	COOL ROOM	Chocovina	7 m ²
2.08	STAIR	Chocovina	13 m ²
2.09	CORRIDOR	Chocovina	35 m ²
2.10	KITCHEN	Chocovina	43 m ²
2.11	COOL ROOM	Chocovina	8 m ²
2.12	FREEZE	Chocovina	8 m ²
2.13	OFFICE	Chocovina	13 m ²
2.14	STAFF WC	Chocovina	8 m ²
2.15	ENT	Share	35 m ²
2.16	FEMALE WC	Chocovina	25 m ²
2.17	ACC. WC	Chocovina	7 m ²
2.18	PHOTOCOPY	Chocovina	6 m ²
2.19	MALE WC	Chocovina	18 m ²
2.20	OFFICE TENANCY	Other	183 m ²
2.21	ACC. WC	Other	7 m ²
2.22	CORRIDOR	Share	14 m ²
2.23	ENT ACC. WC + SHWR	Other	7 m ²
2.24	ENT WC 1	Other	2 m ²
2.25	ENT WC 2	Other	2 m ²
2.26	ENT SHOWER 1	Other	2 m ²
2.27	ENT WC 3	Other	2 m ²
2.28	ENT SHOWER 2	Other	2 m ²
2.29	ENT	Chocovina	6 m ²
2.30	ENT	Chocovina	150 m ²
2.31	BRG	Chocovina	12 m ²
2.32	COFFEE STATION	Chocovina	5 m ²
2.33	CORRIDOR	Chocovina	17 m ²
2.34	LOADING BAY	Share	85 m ²
2.35	BRG	Chocovina	16 m ²
2.36	BRG	Chocovina	8 m ²
2.37	LIFT 01	Chocovina	8 m ²
2.38	LIFT 02	Share	8 m ²
2.39	LIFT 03	Chocovina	8 m ²
2.40	LIFT 04	Chocovina	7 m ²
2.41	STAIR 1	Chocovina	7 m ²
2.42	STAFF STAIR	Chocovina	17 m ²
2.43	STAIR	Share	10 m ²
2.44	STAIR	Share	22 m ²
2.45			1568.43

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2024.08.21	ISSUE FOR INFORMATION	C8
2024.08.16	WIP ISSUE	C7
2024.08.08	LEASE PLAN UPDATE	C6
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BENDIGO SACRED HEART CATHEDRAL SQUARE

Cathedral Square General Arrangement - Level 2

Drawn	Approved	Date	Scale
CT	BM	2024.08.23	1:100 @ A0

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LEVEL 2 FLOOR PLAN



Appendix B

Car Parking Surveys



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Surveyed By: Trans Traffic Survey

Survey Dates & Times: See below

Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023						
			10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm	
ON-STREET CARPARKING															
Map Ref.	WATTLE STREET														
	West Side														
A	Creek Street North to High Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	5	4	5	5	5	5	4	1	1	2	1	0	0
		Motorcycle Parking	2	0	0	0	0	0	0	0	0	0	0	0	0
		1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	3	0	1	1	1	1	0	2	1	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	7	3	4	4	5	4	3	0	1	1	1	0	0
		DDA parking only	1	0	1	1	1	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	14	6	7	8	8	8	7	0	0	0	1	2	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
B	High Street to Mackenzie Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	10	7	9	10	10	9	9	0	0	0	0	0	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	3	0	1	2	1	0	3	0	1	2	1	0	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
C	Mackenzie Street to Rowan Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	9	3	4	4	4	4	4	0	0	0	1	2	
		2P 8:30am-5:30pm Mon-Fri	3	2	1	0	0	0	1	0	1	2	2	1	
		Unrestricted	7	0	1	2	3	3	3	0	0	0	1	2	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	



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Survey Dates & Times: See below

Map Ref.	Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023							
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm		
	WATTLE STREET																
	East Side																
D	Creek Street North to High Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0		
		Unrestricted	5	4	5	5	5	5	5	0	1	2	1	0	0		
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	1	0	1	1	1	1	0	0	0	0	0	0	0		
		Unrestricted	15	12	13	14	15	15	15	0	0	0	1	1	0		
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0		
E	High Street to Mackenzie Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0			
		1 1/2P 9am-5:30pm Mon-Fri	16	8	10	11	12	12	10	3	4	4	4	3	2		
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0		
F	Mackenzie Street to Rowan Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0			
		Unrestricted	18	6	7	8	8	7	6	5	6	6	6	5	4		
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0		
WATTLE STREET			Capacity	112 - 117	117	116	117	117	117	117	117	116	113	113	114	113	112
			Total Number of Cars Parked		55	70	76	79	74	70	70	11	16	19	20	16	7
			Total Number of Vacant Spaces		62	46	41	38	43	47	47	105	97	94	94	97	105
			Percentage Occupancy		47%	60%	65%	68%	63%	60%	60%	9%	14%	17%	18%	14%	6%



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Survey Dates & Times: See below

Map Ref.	Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023					
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm
MACKENZIE STREET															
North Side															
G	View Street to Forest Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	3	0	1	2	2	1	1	3	3	3	3	3	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	15	11	13	14	14	14	14	11	13	15	15	14	12
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
H	Forest Street to Short Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	17	12	15	17	17	17	16	12	15	17	17	16	14
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
I	Short Street to Wattle Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	6	3	4	4	4	4	4	0	1	2	1	0	0
		2P 8:30am-5:30pm Mon-Fri	3	0	1	1	1	1	0	0	0	0	0	0	0
		Unrestricted	3	0	1	1	1	0	0	0	1	1	1	0	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
J	Wattle Street to Vine Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	5	0	1	2	2	2	2	3	3	3	2	0	0
		2P 8:30am-5:30pm Mon-Fri	2	0	1	2	2	2	0	0	0	0	1	1	1
		Unrestricted	4	2	2	2	2	2	0	0	0	0	0	0	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	1	0	1	1	1	1	1	0	0	0	1	1	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0



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Map Ref.	Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023					
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm
MACKENZIE STREET															
South Side															
K	Vine Street to Wattle Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		P10 8:20-9:00am, 3:15-4pm School Days	2	0	0	0	1	1	2	0	0	0	0	0	0
		Unrestricted	6	4	5	5	5	5	6	0	0	0	1	1	1
		2P 8:30am-5:30pm Mon-Fri	2	2	2	2	2	2	2	0	1	2	1	0	0
		Unrestricted	5	4	5	5	5	5	5	0	0	0	1	2	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
L	Wattle Street to Short Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	31	12	15	17	18	18	15	3	3	3	2	0	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
M	Short Street to Forest Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	7	4	5	5	5	5	4	5	6	7	7	6	5
		4P 9am-5:30pm Mon-Fri, 9am-12noon Sat	4	3	4	4	4	4	4	3	4	4	4	4	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
N	Forest Street to View Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	7	6	7	7	7	7	6	5	6	7	7	7	6
		DDA only, 1 1/2P 9am-5:30pm	2	0	0	0	1	1	2	0	1	1	1	1	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	8	6	7	8	8	7	6	7	8	8	8	8	6
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
MACKENZIE STREET			Capacity	131 - 133	133	133	133	133	133	131	133	133	133	133	133
			Total Number of Cars Parked	69	90	99	102	99	88	52	65	73	73	64	56
			Total Number of Vacant Spaces	64	43	34	31	34	43	81	68	60	60	69	77
			Percentage Occupancy	52%	68%	74%	77%	74%	67%	39%	49%	55%	55%	48%	42%



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				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm
	SHORT STREET														
	West Side														
O	Rowan Street to Mackenzie Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	11	10	11	11	11	11	10	6	8	9	8	7	5
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
P	Mackenzie Street to High Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	9	7	8	9	9	9	9	3	4	4	4	3	3
		2P 8:30am-5:30pm Mon-Fri	3	0	1	2	1	0	2	0	0	0	1	1	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		2P 8:30am-5:30pm Mon-Fri	6	4	5	6	6	6	5	3	4	4	4	4	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
Q	High Street to Creek Street North	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	10	3	3	3	3	3	3	1	1	1	1	0	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	



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Survey Dates & Times: See below

Map Ref.	Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023					
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm
SHORT STREET															
East Side															
R	Rowan Street to Mackenzie Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	3	3	3	3	3	3	3	1	1	0	1	1	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		2P 8:30am-5:30pm Mon-Fri	2	2	2	2	2	1	0	0	1	2	2	1	0
		Unrestricted	7	6	7	7	7	7	6	0	0	0	1	2	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
S	Mackenzie Street to High Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	14	10	12	14	14	14	12	6	7	8	7	6	4
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	3	2	3	3	3	3	3	2	3	3	3	3	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1/4P 9am-5:30pm Mon-Fri, 9am-12noon Sat	1	0	1	1	1	0	0	0	0	0	0	0	0
T	High Street to opposite Creek Street North	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		P ticket 8:30am-5:30pm Mon-Fri	10	9	10	10	9	9	9	5	6	6	6	5	4
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
Shorter Street															
U	Both Sides	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
SHORT STREET			Capacity	78 - 79	78	78	78	78	78	78	78	79	79	79	
			Total Number of Cars Parked	56	65	70	69	66	62	27	35	37	38	23	
			Total Number of Vacant Spaces	22	13	8	9	12	16	51	43	42	41	56	
			Percentage Occupancy	72%	83%	90%	88%	85%	79%	35%	45%	47%	48%	29%	



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Surveyed By: Trans Traffic Survey

Survey Dates & Times: See below

Location		Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023					
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm
Map Ref.	ROWAN STREET														
	North Side														
V	Forest Street to Henry Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	11	3	4	4	4	4	3	8	9	10	9	8	7
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
W	Henry Street to Wattle Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	13	9	11	12	12	11	11	4	5	5	5	4	3
		2P 8:30am-5:30pm Mon-Fri	2	0	1	1	1	0	0	2	1	0	0	0	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
South Side															
X	Wattle Street to Short Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		DDA 1 1/2P	1	0	0	0	1	1	0	0	0	0	0	0	0
		P10 9am-9pm, pick up and drop off only	3	0	0	0	0	0	2	0	0	0	0	0	0
		Unrestricted	7	5	6	7	7	6	5	4	5	5	5	4	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
Y	Short Street to Forest Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	13	9	11	13	13	13	11	11	12	13	13	13	10
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
ROWAN STREET		Capacity	47 - 47	47	47	47	47	47	47	47	47	47	47	47	
		Total Number of Cars Parked		26	33	37	38	35	30	29	32	33	32	29	23
		Total Number of Vacant Spaces		21	14	10	9	12	17	18	15	14	15	18	24
		Percentage Occupancy		55%	70%	79%	81%	74%	64%	62%	68%	70%	68%	62%	49%



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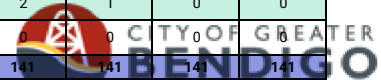
Page: 47 of 54

Surveyed By: Trans Traffic Survey

Survey Dates & Times: See below

Map Ref.	Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023						
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm	
FOREST STREET																
West Side																
Z	Rowan Street to SB #51	No Stopping	-	2	2	1	1	1	0	2	3	3	3	3	3	
		Unrestricted	17	9	11	12	12	12	12	9	11	12	12	12	11	
	SB #51 to Mackenzie Street	Unrestricted	8	9	10	10	10	9	8	8	9	10	10	10	8	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	7	6	7	7	7	7	6	6	7	7	7	7	6	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
AA	Mackenzie Street to High Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
		P10 9am-9pm, pick up and drop off only	1	1	1	0	1	1	1	1	1	1	0	1	1	0
		P ticket 8:30am-5:30pm Mon-Fri	16	12	13	13	13	12	10	13	15	16	16	15	11	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	5	0	1	1	1	0	3	2	2	1	2	2	1	
		DDA only, 2P meter	1	1	1	0	0	0	0	0	0	0	0	0	0	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	8	2	3	3	3	3	3	3	3	3	3	3	2	
		P10 9am-9pm, pick up and drop off only	1	1	1	1	1	1	0	1	1	0	0	0	0	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	3	2	3	3	3	3	3	0	1	2	2	2	1	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
East Side																
BB	Rowan Street to SB #32	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	18	14	16	18	18	18	15	13	16	18	18	18	16	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
		Unrestricted	17	13	15	17	17	17	16	13	15	17	17	17	15	
	SB #32 to Mackenzie Street	Unrestricted	8	7	8	8	8	8	7	6	7	8	8	8	6	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
CC	Mackenzie Street to High Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
		P ticket 8:30am-5:30pm Mon-Fri	11	7	9	10	10	10	10	8	10	11	11	11	10	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	18	5	6	7	7	7	6	11	12	13	12	11	10	
		DDA only, 2P meter	1	0	0	0	1	1	0	0	0	0	1	1	1	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	2	0	1	2	1	0	0	0	1	2	1	0	0	
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0	
FOREST STREET			Capacity	141 - 141	141	141	141	141	141	141	141	141	141	141		
			Total Number of Cars Parked	90	107	113	113	109	99	95	113	123	120	101		
			Total Number of Vacant Spaces	51	34	28	28	32	42	46	28	18	21	40		
			Percentage Occupancy	64%	76%	80%	80%	77%	70%	67%	80%	87%	87%	72%		

Nb/Sb - Northern/Southern Property Boundary
 Eb/Wb - Eastern/Western Property Boundary
 Mid pt - Mid point
 ROW - Right of Way



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Surveyed By: Trans Traffic Survey

Survey Dates & Times: See below

Location	Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023						
			10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm	
Map Ref. HENRY STREET															
West Side															
DD	Rowan Street to Richard Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	3	1	1	1	1	1	1	1	1	1	1	1	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
East Side															
	Rowan Street to Richard Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
HENRY STREET		Capacity	3 - 3	3	3	3	3	3	3	3	3	3	3	3	
		Total Number of Cars Parked		1	1	1	1	1	1	1	1	1	1	1	
		Total Number of Vacant Spaces		2	2	2	2	2	2	2	2	2	2	2	
		Percentage Occupancy		33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	33%	
Map Ref. HIGH STREET															
North Side															
EE	Vine Street to Wattle Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	2	0	0	0	1	2	2	2	2	1	1	0	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	4	1	1	2	1	2	3	0	0	0	1	1	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
FF	Wattle Street to Short Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	12	7	8	8	8	8	8	0	0	0	1	1	0
		P10 pick up and drop off only	1	1	1	0	1	1	0	1	1	0	1	1	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
GG	Short Street to Forest Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	4	3	3	3	3	3	3	2	3	3	3	3	3
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	6	4	5	5	5	5	5	3	4	4	4	4	4
		P10 pick up and drop off only	1	0	0	0	1	1	1	0	1	1	1	1	0
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	1	0	0	0	0	0	1	1	1	0	1	1	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	



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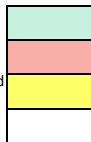
Survey Dates & Times: See below


Location		Restriction	Capacity Min - Max	Friday 5th May, 2023						Saturday 6th May, 2023					
				10am	11am	12pm	1pm	2pm	3pm	10am	11am	12pm	1pm	2pm	3pm
Map Ref.	HIGH STREET														
	South Side														
HH	Vine Street to Wattle Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Unrestricted	2	0	0	0	0	0	0	0	0	0	0	0	0
		1 1/2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	3	1	1	0	0	0	0	2	3	3	2	1	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
II	Wattle Street to Short Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	2	1	1	1	2	2	2	0	1	1	1	0	2
		1/4P	1	0	0	0	1	1	0	1	1	1	1	0	0
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		2P 9am-5:30pm Mon-Fri, 9am-12noon Sat	2	0	0	0	0	0	2	2	2	2	1	0	2
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
JJ	Short Street to opposite Forest Street	No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Bus Zone 6am-11pm, Taxi Zone 11pm-6am	2	0	0	0	1	2	0	2	2	2	1	0	0
		P10 pick up and drop off only	1	0	1	1	1	0	0	0	1	1	1	0	1
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	1	0	0	0	0	0	0	0	0	0	1	1	1
		No Stopping	-	0	0	0	0	0	0	0	0	0	0	0	0
		Loading Zone 15min	1	1	1	0	1	1	0	0	1	1	1	0	1
		2P Ticket 8:30am-5:30pm Mon-Fri, 2P 8:30am-5:30pm Sat	6	5	6	6	6	6	6	3	4	4	4	3	2
HIGH STREET		Capacity	45 - 45	45	45	45	45	45	45	45	45	45	45	45	
		Total Number of Cars Parked	22	25	25	26	28	32	15	20	18	20	15	16	
		Total Number of Vacant Spaces	23	20	20	19	17	13	30	25	27	25	30	29	
		Percentage Occupancy	49%	56%	56%	58%	62%	71%	33%	44%	40%	44%	33%	36%	
SUMMARY => ON-STREET CARPARKING															
Car Parking Supply		560 - 564	564	563	564	564	564	564	562	563	560	561	562	561	560
Total Number of Cars Parked			319	391	421	428	412	382	230	282	304	307	278	227	
Total Number of Vacant Spaces			245	172	143	136	152	180	333	278	257	255	283	333	
Percentage Occupancy			57%	69%	75%	76%	73%	68%	41%	50%	54%	55%	50%	41%	

Note: Public parking includes spaces that are available to the general public and excludes 'No Stopping', 'P10', '1/4P', 'Loading Zones' and 'Bus Zones' areas, etc., during the relevant enforcement periods

LEGEND: Public Parking

- Not available to the general public
- Not Available, illegally parked cars included in analysis
- No Stopping/
- Other No Parking

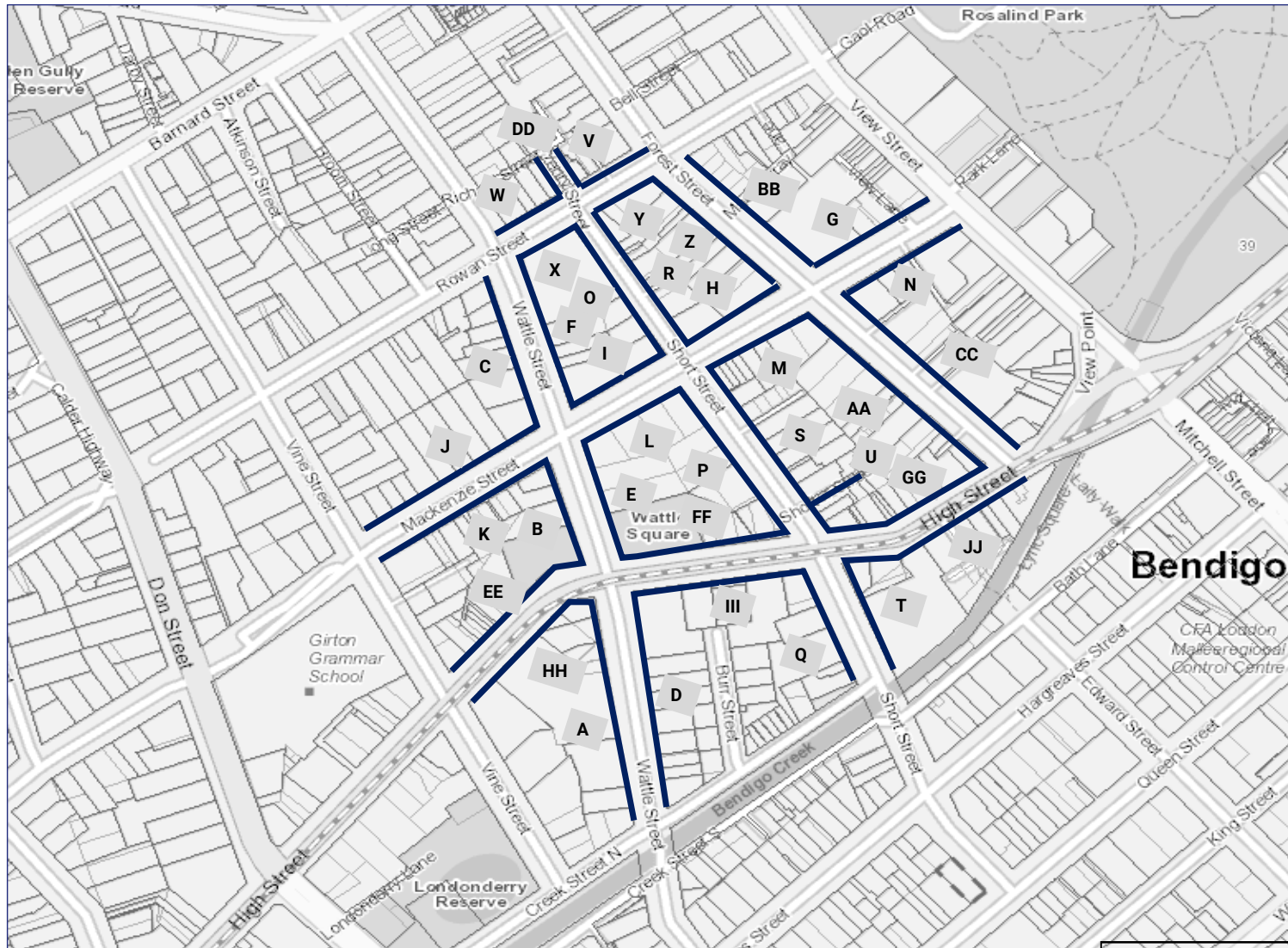




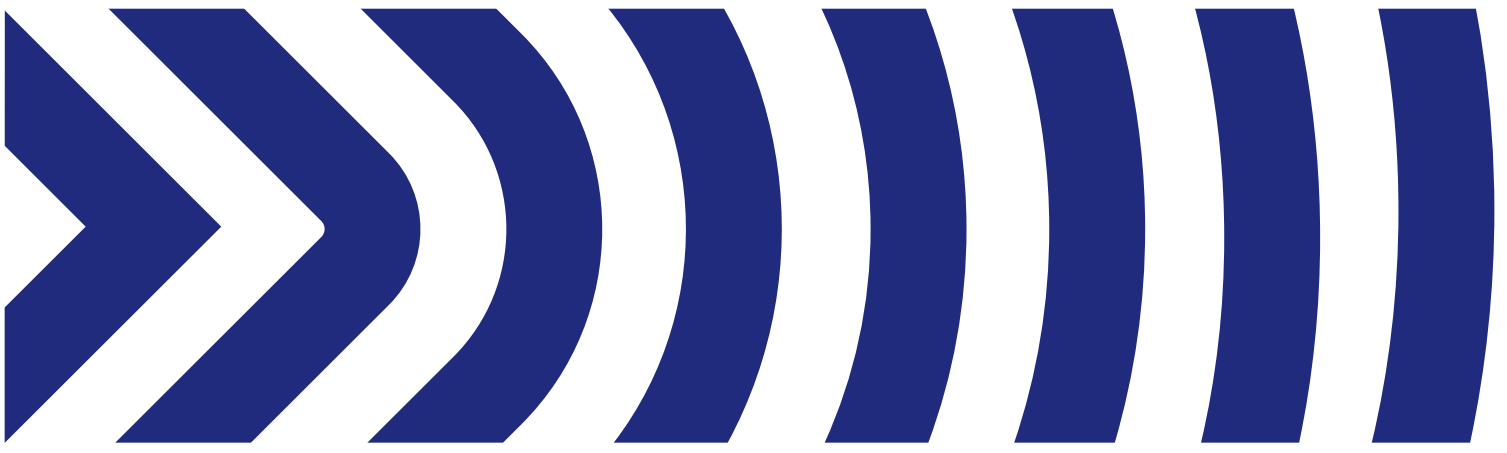
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Appendix C

Swept Path Diagrams

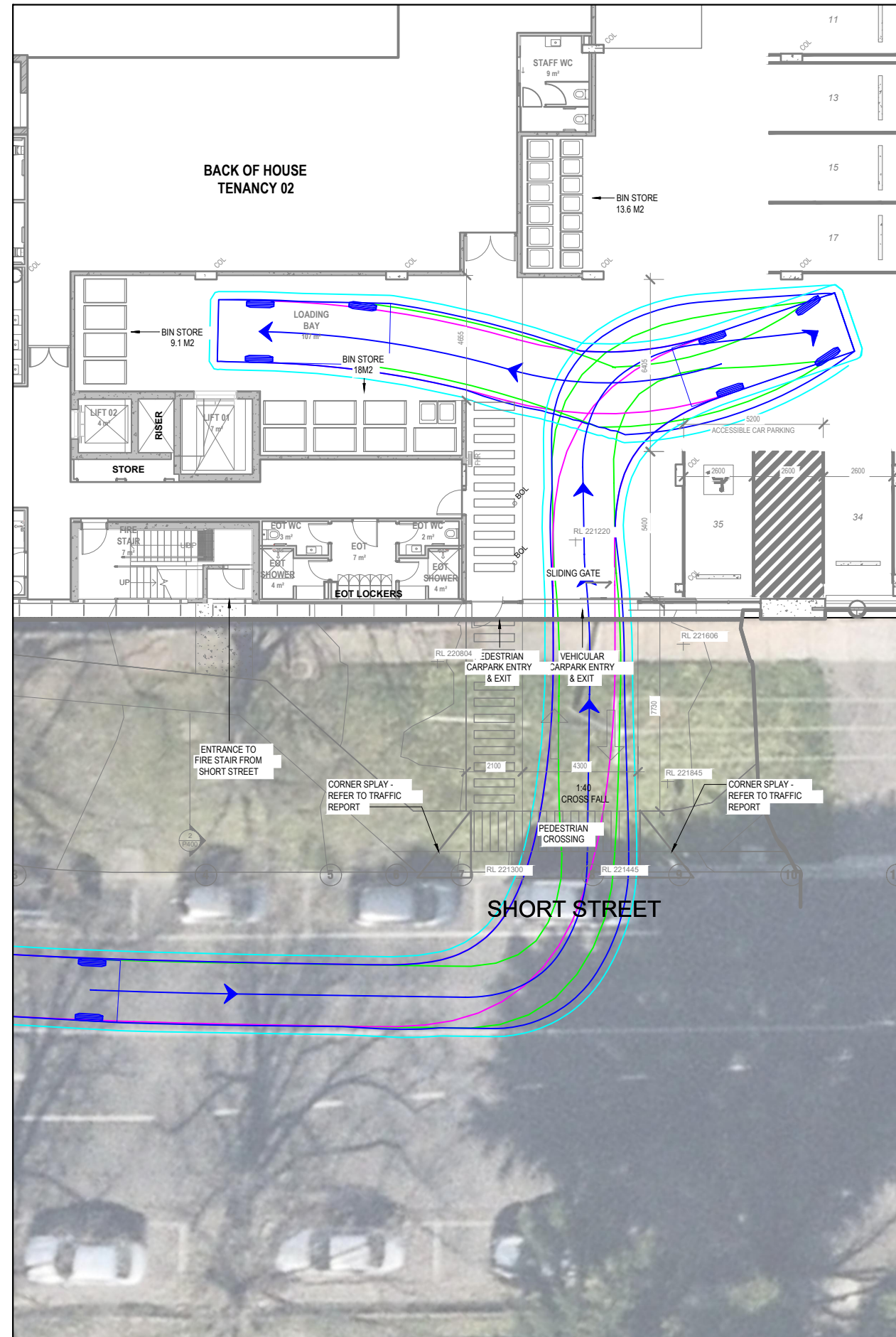


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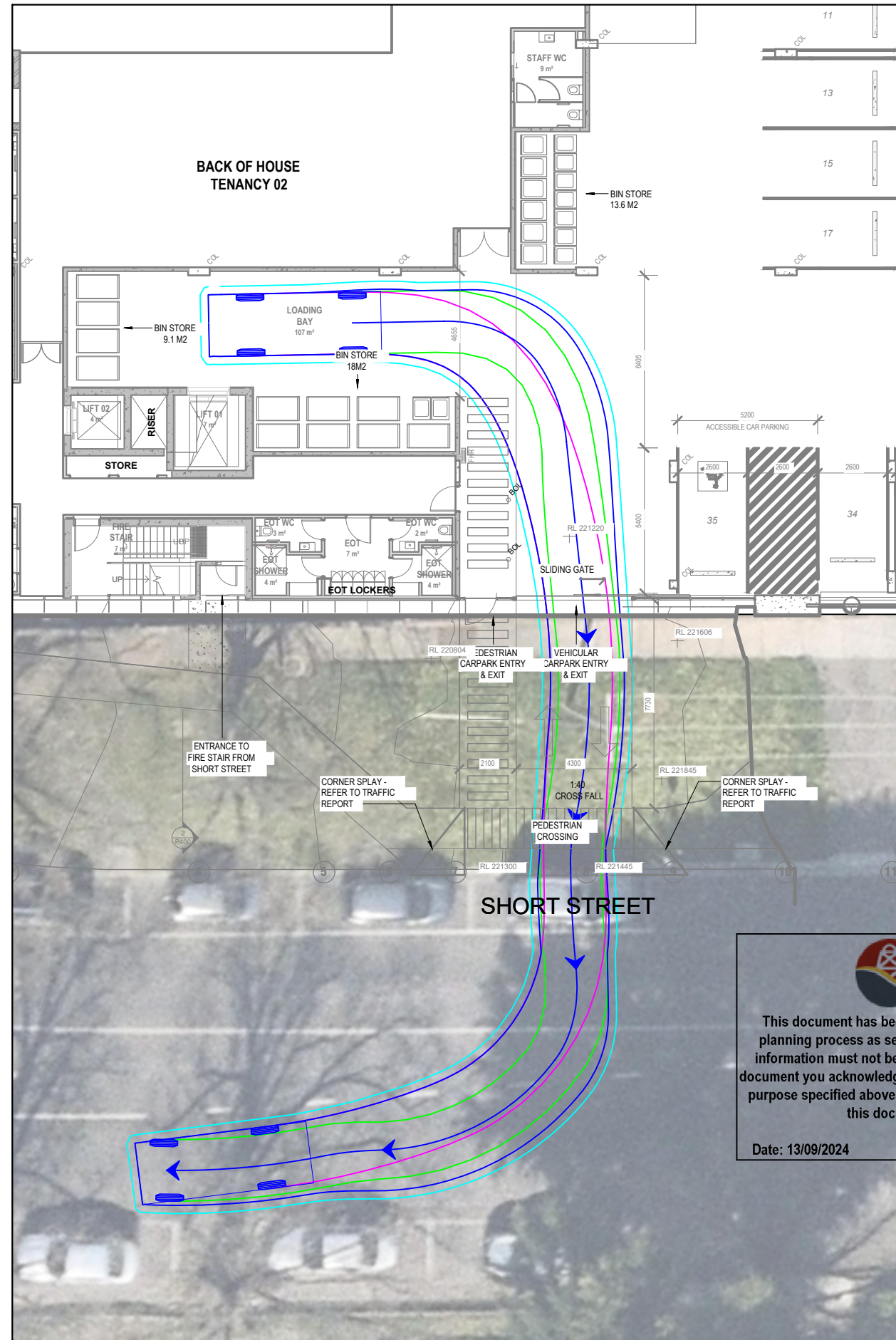
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6.4m SRV - INGRESS

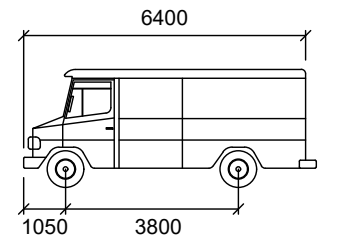


6.4m SRV - EGRESS




VEHICLE PROFILE

VEHICLE USED IN SIMULATION
(VEHICLE SPEED - 5KM/H)



SRV (AS 2890.2) mm
 Width : 2300
 Track : 2300
 Lock to Lock Time : 6.0
 Steering Angle : 38.0

LEGEND
 REAR WHEELS (pink line)
 FRONT WHEELS (green line)
 VEHICLE BODY (blue line)
 BODY CLEARANCE (cyan line)

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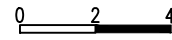
REV	DATE	NOTES	DESIGNED BY	CHECKED BY
A	26/03/2024	TRAFFIC ENG	J. YOUNG	L. FURNESS

CATHOLIC DIOCESE OF SANDHURST
 PROPOSED COMMERCIAL DEVELOPMENT

GENERAL NOTES:
 BASE INFORMATION: 1892 Bendigo Sacred Heart - P201 - Floor Plan - Level 2.dwg
 DRAWINGS BY: Bickerton Masters

FILE NAME: G31621
 SHEET NO.: 01

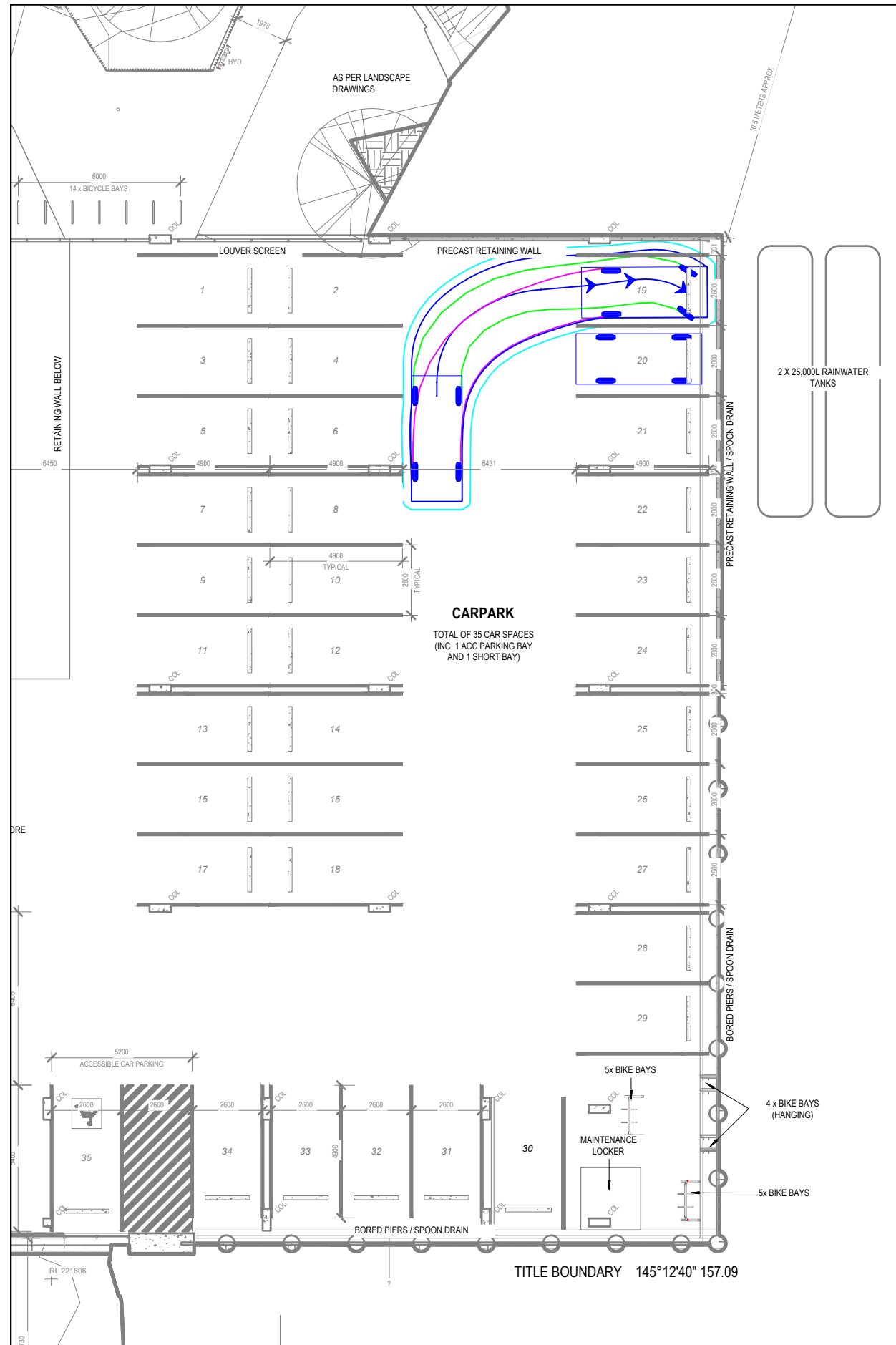


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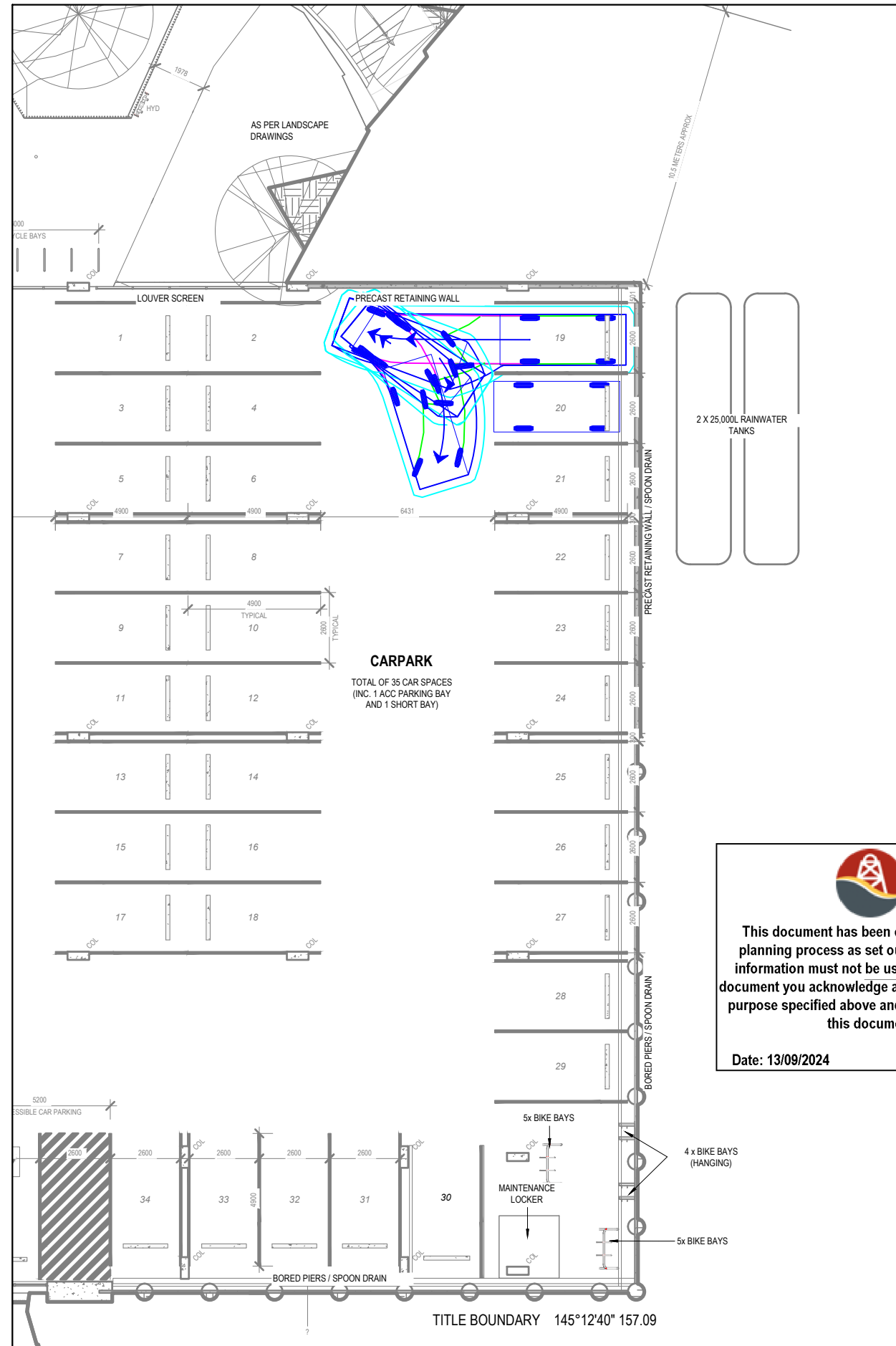
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 T: (03) 9822 2888
 www.trafficgroup.com.au

END CAR SPACE - INGRESS

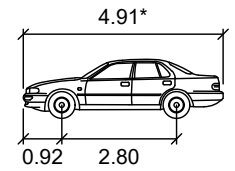


END CAR SPACE - EGRESS



VEHICLE PROFILE

VEHICLE USED IN SIMULATION
(VEHICLE SPEED - 5KM/H)



85th percentile
(AS/NZS 2890.1:2004)

Width : 1.87m
Track : 1.77m
Kerb to Kerb Radius : 11.5m

* actual template based on 'relevant longitudinal dimensions that affect swept path' as set out in Section B2.1 of AS/NZS 2890.1:2004

LEGEND
 REAR WHEELS (pink line)
 FRONT WHEELS (green line)
 VEHICLE BODY (blue line)
 BODY CLEARANCE (cyan line)



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Date: 13/09/2024

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REV	DATE	NOTES	DESIGNED BY	CHECKED BY
A	26/03/2024	TRAFFIC ENG	J. YOUNG	L. FURNESS

CATHOLIC DIOCESE OF SANDHURST
PROPOSED COMMERCIAL DEVELOPMENT

GENERAL NOTES:
BASE INFORMATION: 1892 Bendigo Sacred Heart - P201 - Floor Plan - Level 2.dwg
DRAWINGS BY: Bickerton Masters

FILE NAME: G31621
SHEET NO.: 02



SCALE: 1:200 (A3)

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